

**BOARD OF APPEALS, CITY & COUNTY OF SAN FRANCISCO**

Appeal of  
CATHERINE WOODS, \_\_\_\_\_ )  
Appellant(s) )  
vs. )  
SAN FRANCISCO PUBLIC WORKS )  
BUREAU OF STREET USE & MAPPING, \_\_\_\_\_ )  
Respondent

Appeal No. **24-050**

**NOTICE OF APPEAL**

**NOTICE IS HEREBY GIVEN THAT** on August 29, 2024, the above named appellant(s) filed an appeal with the Board of Appeals of the City and County of San Francisco from the decision or order of the above named department(s), commission, or officer.

The substance or effect of the decision or order appealed from is the ISSUANCE on July 17, 2024 to Randall Laroche, of a Street Improvement Permit (remove and reconstruct new ten-foot driveway curb cut and sidewalk per approved plan, saw-cut two-feet of AC gutter and replace with two inches ACWS over eight-inch concrete base; additional paving as required and directed by SFPW/BSM inspector; field inspection is mandatory prior to excavation and pouring concrete) at 551 Alvarado Street.

**APPLICATION NO. 23IE-00435**

**FOR HEARING ON October 9, 2024**

Address of Appellant(s):

Address of Other Parties:

<p>Catherine Woods, Appellant(s) 555 Alvarado Street San Francisco, CA 94114</p>	<p>Randall Laroche, Permit Holder(s) c/o Amy Lee, Agent for Permit Holder(s) 3S LLC 3053 Fillmore Street #191 San Francisco, CA 94123</p>
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Date Filed: August 29, 2024

**CITY & COUNTY OF SAN FRANCISCO  
BOARD OF APPEALS**

**PRELIMINARY STATEMENT FOR APPEAL NO. 24-050**

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I / We, **Catherine Woods**, hereby appeal the following departmental action: **ISSUANCE** of **Street Improvement Permit No. 23IE-00435** by the **San Francisco Public Works, Bureau of Street Use & Mapping** which was issued or became effective on: **July 17, 2024**, to: **Randall Laroche**, for the property located at: **551 Alvarado Street**. [Note: On August 28, 2024, the Board granted the appellant's request for the Board to take jurisdiction over the above-referenced permit].

**BRIEFING SCHEDULE:**

Appellant's Brief is due on or before: 4:30 p.m. on **September 19, 2024, (no later than three Thursdays prior to the hearing date)**. The brief may be up to 12 pages in length with unlimited exhibits. It shall be double-spaced with a minimum 12-point font. An electronic copy shall be emailed to: [boardofappeals@sfgov.org](mailto:boardofappeals@sfgov.org), [julie.rosenberg@sfgov.org](mailto:julie.rosenberg@sfgov.org), [vincent@vgonzaga.com](mailto:vincent@vgonzaga.com); [amy@3ssanfrancisco.com](mailto:amy@3ssanfrancisco.com); and [nicolas.huff@sfdpw.org](mailto:nicolas.huff@sfdpw.org)

Respondent's and Other Parties' Briefs are due on or before: 4:30 p.m. on **October 3, 2024, (no later than one Thursday prior to hearing date)**. The brief may be up to 12 pages in length with unlimited exhibits. It shall be doubled-spaced with a minimum 12-point font. An electronic copy shall be emailed to: [boardofappeals@sfgov.org](mailto:boardofappeals@sfgov.org), [julie.rosenberg@sfgov.org](mailto:julie.rosenberg@sfgov.org), [Nicolas.huff@sfdpw.org](mailto:Nicolas.huff@sfdpw.org), and [cathywoods@gmail.com](mailto:cathywoods@gmail.com)

Hard copies of the briefs do NOT need to be submitted to the Board Office or to the other parties.

Hearing Date: **Wednesday, October 9, 2024, 5:00 p.m., Room 416 San Francisco City Hall, 1 Dr. Carlton B. Goodlett Place**. The parties may also attend remotely via Zoom. Information for access to the hearing will be provided before the hearing date.

All parties to this appeal must adhere to the briefing schedule above, however if the hearing date is changed, the briefing schedule MAY also be changed. Written notice will be provided of any changes to the briefing schedule.

In order to have their documents sent to the Board members prior to hearing, **members of the public** should email all documents of support/opposition no later than one Thursday prior to hearing date by 4:30 p.m. to [boardofappeals@sfgov.org](mailto:boardofappeals@sfgov.org). Please note that names and contact information included in submittals from members of the public will become part of the public record. Submittals from members of the public may be made anonymously.

**Please note** that in addition to the parties' briefs, any materials that the Board receives relevant to this appeal, including letters of support/opposition from members of the public, are distributed to Board members prior to hearing. All such materials are available for inspection on the Board's website at [www.sfgov.org/boa](http://www.sfgov.org/boa). You may also request a hard copy of the hearing materials that are provided to Board members at a cost of 10 cents per page, per S.F. Admin. Code Ch. 67.28.

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**The reasons for this appeal are as follows:** See attached.

**Appellant or Agent:**

Signature: Via Email

Print Name: Catherine Woods, appellant

I wish to file an appeal against the curb cut as proposed by the permit 23IE-00435. The curb cut would take 2 parking spots off a street where parking is extremely tight, in exchange for one off-street private parking garage.

The curb cut would result in a 12' 7" space on one side of the proposed curb cut, and an 11' 7" space on the other. Neither of which is sufficient for parking a standard compact car. For context a compact car is considered to be between 13' 3" and 15' 7".

On our block, most of the houses are 25' wide with a 9' curb cut, if they have a garage, resulting in a street parking space of 16', or in some cases less.

A new law, AB 413 (Lee), prohibits the stopping, standing or parking of a vehicle within 20 feet of any crosswalk, whether it's marked or not, and within 15 feet of a crosswalk with a curb extension, so the block will lose an additional 3-4 parking spots making parking even more difficult on our block than it already is.

I would like to ask that the Board of Appeals to reverse the decision made by DPW to issue permit 23IE-00435 with the curb cut location as proposed.



# City and County of San Francisco

San Francisco Public Works - Bureau of Street Use and Mapping  
49 South Van Ness Ave, Suite 300 - San Francisco, CA 94103  
sfpublicworks.org - tel (628) 271-2000



**23IE-00435**

## Street Improvement Permit

**Address : 551 ALVARADO ST**

**Cost: \$1,827.50**

**Block:3624 Lot: 039 Zip: 94114**

Pursuant to article 2.4 of the Public Works Code in conjunction to DPW Order 187,005, permission, revocable at the will of the Director of Public Works, to construct improvements within the public right-of-way is granted to Permittee. Major events in San Francisco the week of November 12, 2023, may necessitate street closures and traffic rerouting. As a result, Public Works-issued permits allowing work in the public right of way may be suspended temporarily per Public Works Code section 2.4.54 and section 724(f), starting as early as November 1, 2023. Once we have more details on the potential impacts, Public Works will contact you if your permit will be affected. Thank you for your understanding.

### JS Concrete Construction

**Name:** JS Concrete Construction

#### Conditions

Permittee shall coordinate all work with other construction projects and events known or unforeseen such as to minimize the impact of construction project on the general public and/or event(s).

Mandatory Coordination with conflicting permits is required. Permit holder shall not commence work without first properly coordinating with existing permit holders as noted on the exception page(s) of this permit. If this permit conflicts with a city project or other approved permits, the permit holder of this permit shall be responsible for proper coordination and evaluation of the site prior to commencing work.

The permittee shall comply with all existing traffic controls and parking restrictions. The permittee shall also comply with any additional restrictions under the Special Traffic Permit issued by SFMTA. For information related to construction traffic restrictions please reference the latest edition of Regulations for Working in San Francisco Streets, the Blue Book. To download a copy of the Blue Book, please visit <https://www.sfmta.com/services/streets-sidewalks/construction-regulations>.

A separate Street Space Permit is required for construction staging.

**NTR** 0

**Curb Cut Sq Footage** 30

**Completion** Work shall be completed, inspected and signed off by inspector by the expiration date

<b>Remove, replace or reconstruct:</b>	Remove and reconstruct new (10') driveway curb cut and sidewalk per approved plan, saw-cut 2' of AC gutter and replace with 2" ACWS over 8" concrete base. Additional paving as required and directed by SFPW/BSM inspector. Field inspection is MANDATORY prior to excavation and pouring concrete, call 1-628-271-2000 or email dpw-bsminspects@sfdpw.org to schedule.
<b>Expiration Date</b>	9/15/2025
<b>Bond Amount:</b>	0
<b>Linear Footage</b>	0
<b>Bond Holder:</b>	
<b>Contact</b>	Refer to Agent
<b>DPW Resolution #</b>	
<b>Inspection</b>	Work shall not commence until this permit has been activated by Public Works. The permittee shall contact Public Works at dpw-bsminspects@sfdpw.org or (628) 271-2000 to activate the permit and schedule inspection at least 72 hours prior to work. Failure to follow the activation process prior to commencing work may result in a correction notice and possible notice of violation.

The undersigned Permittee hereby agrees to comply with all requirements and conditions noted on this permit

**Approved Date : 07/17/2024**

Excavation and grading of subject area for street reconstruction shall be in accordance with approved plans and City specifications. Damaged areas adjacent to this construction shall be properly patched per City Inspector. Also, the permittee shall be responsible for any ponding due to the permitted work.

\_\_\_\_\_  
Applicant/Permittee

\_\_\_\_\_  
Date

Distribution:  
Outside BSM: BOE (Streets and Hyws) - P. Riviera  
Inside BSM: Street Improvement Inspection

**Printed : 7/17/2024 9:25:24 PM**

Plan Checker

Berhane Gaime

## STREET EXCAVATION REQUIREMENTS

1. The permittee shall call Underground Service Alert (U.S.A.), telephone number 811, 48 hours prior to any excavation.
2. All work including sidewalk and pavement cutting and removal, lagging, excavation, backfill, and sidewalk and pavement restoration shall be done by a licensed paving contractor and in accordance with the requirements of the Current Standard Specifications of Public Works.
3. All work including sidewalk and pavement cutting and removal, lagging, excavation, backfill, and sidewalk and pavement restoration shall be done by a licensed contractor and in accordance with the requirements of the latest edition of Standard Specifications and Plans of San Francisco Public Works, and Department of Public Works Order Nos. 187,005.
4. Sidewalk and pavement restoration shall include the replacement of traffic lane and crosswalk striping, parking stall markings, and curb painting that might have been obliterated during street excavation. The permittee shall perform their work under on the following options:
  - a. Have the City forces do the striping and painting work at the permittees expense. The permittee shall make a deposit with the Department of Parking & Traffic for this purpose in an amount estimated by the Municipal Transportation Agency (MTA) 7th Floor 1 South Van Ness Ave telephone 701-4500, and notify the MTA at least 48 hours in advance of the time the work is to be done.
  - b. Perform the work themselves following instructions available at the Department of Parking & Traffic and MTA.
5. The permittee shall submit a non-refundable fee to Bureau of Street-Use and Mapping to pay for City Inspection of the backfill and pavement restoration. At least 48 hours in advance, the permittee shall make arrangements with the Street Improvement Section Inspectors, (628) 271-2000, for an inspection schedule.
6. The permittee shall file and maintain an excavation bond in the sum of \$25,000.00 with the Department of Public Works, to guarantee the maintenance of the pavement in the excavation area for a period of 3 years following the completion of the backfill and pavement restoration pursuant to Article 2.4.40 of the Public Works Code.
7. The permittee shall conduct construction operations in accordance with the requirements of Article 900 Section 903(a) and (b) of the Traffic Code. The permittee shall contact the MTA 7th Floor 1 South Van Ness Ave telephone 701-4500, for specific restrictions before starting work.
8. The permittee shall obtain the required permits, if any, from regulating agencies of the State of California.
9. The permittee shall verify the locations of any City or public service utility company facilities that may be affected by the work authorized by this permit and shall assume all responsibility for any damage to such facilities. The permittee shall make satisfactory arrangements and payments for any necessary temporary relocation of City or public utility company facilities.
10. The permittee shall pay the required fee for sewer installation permit at the Plumbing Inspection Division, Department of Building Inspection, 1660 Mission Street and arrange for inspection of this work, telephone 558-6054.
11. Planting of trees and performance of any work in the right-of-way which may affect a tree and/or landscaping shall not be performed prior to obtaining a permit and/or another form of approval from Bureau of Urban Forestry (BUF), telephone: (628) 652-8733.
12. Per DPW Order 201,954, the recycling of Cobble Stones and Granit Curb shall follow as:
  - a. Cobblestones shall be clean of dirt prior to transporting. Extreme care shall be taken during the transporting the cobblestones to minimize damage before delivery to City. The cobblestones shall be neatly and securely placed on pallets so they can be moved about safely after the delivery. The Minimum size of cobblestone shall be 4 inches square (16 square inches). The cobblestones shall be delivered, including off loading, to 701 14th Street on Treasure Island or at alternative location directed by the Department within the City of San Francisco. Contact the Department forty-eight hours (48 hours) prior to delivery. The Department can be reached at (415) 641-2627.
  - b. Granite Curb shall be neatly and securely placed on pallets so they can be moved about safely after delivery. The Contractor shall exercise care in transporting the granite curb to minimize damage. The length limit of recyclable granite curbs shall be no less than four feet. The granite curb shall be delivered, including off loading, to 701 14th Street on Treasure Island or at an alternative location directed by the Department within the City of San Francisco. Contact Bureau of Street and Sewer Repair (BSSR) at least forty-eight hours (48 hours) prior to delivery. BSSR can be reached at (415) 695-2087.
13. In consideration of this Permit being issued for the work described in the application, Permittee on its behalf and that of any successor or assign, and on behalf of any lessee, promises and agrees to perform all the terms of this Permit and to comply with all applicable laws, ordinances and regulations.
14. Permittee agrees on its behalf and that of any successor or assign to hold harmless, defend, and indemnify the City and County of San Francisco, including, without limitation, each of its commissions, departments, officers, agents and employees (hereinafter collectively referred to as the "City") from and against any and all losses, liabilities, expenses, claims, demands, injuries, damages, fines, penalties, costs or judgments including, without limitation, attorneys' fees and costs (collectively, "claims") of any kind allegedly arising directly or indirectly from (i) any act by, omission by, or negligence of, Permittee or its subcontractors, or the officers, agents, or employees of either, while engaged in the performance of the work authorized by this Permit, or while in or about the property subject to this Permit for any reason connected in any way whatsoever with the performance of the work authorized by this Permit, or allegedly resulting directly or indirectly from the maintenance or installation of any equipment, facilities or structures authorized under this Permit, (ii) any accident or injury to any contractor or subcontractor, or any officer, agent, or employee of either of them, while engaged in the performance of the work authorized by this Permit, or while in or about the property, for any reason connected with the performance of the work authorized by this Permit, or arising from liens or claims for services rendered or labor or materials furnished in or for the performance of the work authorized by this Permit, (iii) injuries or damages to real or personal property, good will, and persons in, upon or in any way allegedly connected with the work authorized by this Permit from any cause or claims arising at any time, and (iv) any release or discharge, or threatened release or discharge, of any hazardous material caused or allowed by Permittee in, under, on or about the property subject to this Permit or into the environment. As used herein, "hazardous material" means any substance, waste or material which, because of its quantity, concentration of physical or chemical characteristics is deemed by any federal, state, or local governmental authority to pose a present or potential hazard to human health or safety or to the environment.
15. Permittee must hold harmless, indemnify and defend the City regardless of the alleged negligence of the City or any other party, except only for claims resulting directly from the sole negligence or willful misconduct of the City. Permittee specifically acknowledges and agrees that it has an immediate and independent obligation to defend the City from any claim which actually or potentially falls within this indemnity provision, even if the allegations are or may be groundless, false or fraudulent, which obligation arises at the time such claim is tendered to Permittee by the City and continues at all times thereafter. Permittee agrees that the indemnification obligations assumed under this Permit shall survive expiration of the Permit or completion of work.
16. Permittee shall obtain and maintain through the terms of this Permit general liability, automobile liability or workers' compensation insurance as the City deems necessary to protect the City against claims for damages for personal injury, accidental death and property damage allegedly arising from any work done under this Permit. Such insurance shall in no way limit Permittee's indemnity hereunder. Certificates of insurance, in form and with insurers satisfactory to the City, evidencing all coverages above shall be furnished to the City before commencing any operations under this Permit, with complete copies of policies furnished promptly upon City request.
17. The permittee and any permitted successor or assign recognize and understand that this permit may create a possessory interest.
18. Separate permit is required for excavation of side sewers. Installation authorized only by Class "A" or "C-42" Licensed Contractor or "C-12" with "C-36" Licensed Contractor. Authorization requires the filing of a \$25,000 excavation bond to cover the cost of City inspection and having obtained authorization to excavate in the roadway. The contractor shall obtain the proper permits and arrange for an inspection, for the section of pipe from the trap to the property, with the Plumbing Inspection Division at 1660 Mission Street, telephone 558-6054.
19. Pursuant to state law, all survey monuments must be preserved. No work (including saw cutting) may commence within 20' of a survey monument until an application for Monument Referencing has been approved and notification of monument referencing has occurred. Prior to construction, all CCSF survey monuments shall be referenced by a licensed Land Surveyor on a Corner Record or Record of Survey if any construction will take place within 20 ft. of a monument. For any questions please email Monument.Preservation@sfdpw.org or call 415-554-5827. Note, all survey monuments shall be preserved per state law and disturbance of a survey monument is a crime.

Not all survey monuments are visible.

20. If sidewalk finish grade is to be altered, permittee shall replace, relocate and reset pull boxes as necessary to ensure pull boxes are flush with sidewalk, not damaged and not covered, filled or sprayed with concrete and debris, etc. Permittee shall also raise or lower street light, signal poles and other city poles/infrastructure as necessary to ensure anchor bolts and hand holes are not buried or that base of pole is elevated above sidewalk surface. For additional guidance street light and signal poles matters, consult Transportation Engineer Norman Wong at [norman.wong@sfmta.com](mailto:norman.wong@sfmta.com).

# Permit Addresses

23IE-00435

\*RW = RockWheel, SMC = Surface Mounted Cabinets, S/W = Sidewalk Work, DB = Directional Boring,  
 BP= Reinforced Concrete Bus Pad, UB = Reinforced Concrete for Utility Pull Boxes and Curb Ramps  
 Green background: Staging Only

Number of blocks: 1    Total repair size:0 sqft    Total Streetspace:0    Total Sidewalk: sqft

ID	Street Name	From St	To St	Sides	*Other	Asphalt	Concrete	Street Space Feet	Sidewalk Feet
1	ALVARADO ST	NOE ST	CASTRO ST	Odd	RW : False SMC : False S/W Only : False DB: False BP: False UB: False	0	0	0	
<b>Total</b>						<b>0</b>	<b>0</b>	<b>0</b>	



# Exceptions

23IE-00435

Street Name	From St	To St	Message	Job	Contact	Dates
ALVARADO ST						
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	12MSE-0298	Refer to Agent - Refer to Agent	
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	14CN-0143	415 309 0107 - 415 309 0107	
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	14ECN-0088	800-743-5000 - 800-743-5000	Jan 14 2014-
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	20CN-00111	(843) 709-2136 - (843) 709- 2136	
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	20CN-00133	Refer to Agent - Refer to Agent	
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	20MSE-00492	Refer to Agent - Refer to Agent	
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	21ECN-00088	Refer to Agent 415 550 4956 - 415-550-4900	Feb 2 2021-
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	21ECN-00094	Refer to Agent 415 550 4956 - 415-550-4900	Feb 3 2021-
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	23ECN-00124	Oscar Delgadillo @ 510-867- 0073 - (415) 337-2934	Feb 16 2023-
	NOE ST	CASTRO ST -	Proposed Excavation.	SF DPW IDC - Hydraulic	Satnam Kaur -	Nov 15 2024-Aug 25 2025

# **BRIEF(S) SUBMITTED BY APPELLANT(S)**

I wish to file an appeal against the curb cut as proposed by the permit 23IE-00435. The curb cut would remove two public parking spots from a street, where parking is very tight, in exchange for one off-street private parking garage.

I live next door to 551 Alvarado St., where the project sponsors are carrying out extensive renovations. I'm very excited to see the property being cleaned up. The project immediately to the right me, 559 Alvarado St., was renovated in 2020-2022, to the same enthusiasm on my part. All the properties on our block increase in value when renovations like these are done.

However, I object to the project taking 2 parking spots off the street in exchange for 1 car parking. The proposed curb cut would result in a 12' 7" space on one side of the curb cut, and an 11' 7" space on the other. Neither of which is sufficient for parking a standard compact car. For context a compact car is considered to be between 13' 3" and 15' 7". (See exhibit 1)

On our block, most of the houses are 25 feet wide with a 9 foot curb cut, if they have a garage, resulting in a standard street parking space of 16', or in some cases less. (See exhibit 2)

Two houses down the street from 551 Alvarado St. are the two buildings 535 & 545 Alvarado St., containing six residential units, from 1-3 bedrooms, all with no off-street parking. Next to those is a two-unit residential building, also with no off-street parking. This results in approximately 88 feet of street parking to accommodate parking for eight units. The maximum number of compact cars that can fit in that space is five. Using the math that the occupiers of each unit will have just one car, that section of road is 3 parking spots short.

The 551 renovation project will also have 2 units, with a parking garage that can accommodate only one car. Using the same math that the occupiers of each unit will have just one car, the proposed curb cut would result in another parking space short.

A new law, AB 413 (Lee), prohibits the stopping, standing or parking of a vehicle within 20 feet of any crosswalk, whether it's marked or not, and within 15 feet of a crosswalk with a curb extension, so the block will lose an additional 3-4 parking spots making parking even more difficult on our block than it already is. (See exhibit 3)

The project sponsors have repeatedly insisted that the curb cut would only take one parking spot from the street. On March 7th, 2024 they wrote to the 500-Alvarado block group: "Our project will create two dwelling units with off street parking. There is a large deteriorating tree in front of the property and in order to accommodate off street parking, a curb cut will be made in front of the property; there will likely be a loss of one standard-size street parking space." (See exhibit 4)

On March 11th, 2024 they wrote: "The project would eliminate one space, not two as is being alleged, and would provide in exchange parking for the future inhabitants of the house, possibly ourselves." (See exhibit 5)

On March 11th, 2024, I wrote to the project sponsors and DPW, along with a diagram: "Randy & David, Unless you have changed your plans, and please let us know if you did, they remain, as

is, on the city website. Presently, there is approximately 34'-2" available for street parking outside 551 Alvarado St., which can fit 2 compact cars. You are proposing to put a curb cut smack bang in the middle of that as shown below. (diagram) Please have your architect draw me a diagram showing me how I am incorrect.” (See exhibit 6)

To which I received no reply. When the project sponsors were informed of the fact that 2 parking spots would be lost as a result of their curb cut, in March 2024, long before the permit for the street improvement was granted by DPW, they chose to ignore it, prioritizing private interests over the neighborhood community. This is why I have been forced to file this appeal, as I informed everyone many times in email exchanges.

Many of my neighbors on the block also support my position on this. (See exhibit 7)

On page 3 of the San Francisco Planning Department’s “Guidelines for Adding Garages and Curb Cuts”, under the heading PLACEMENT OF THE GARAGE AND CURB CUT, a question is posed about on-street parking: “Could a greater number of on-street parking spaces be retained if the curb cut and garage were shifted elsewhere on the building’s frontage?” (See exhibit 8 & 9)

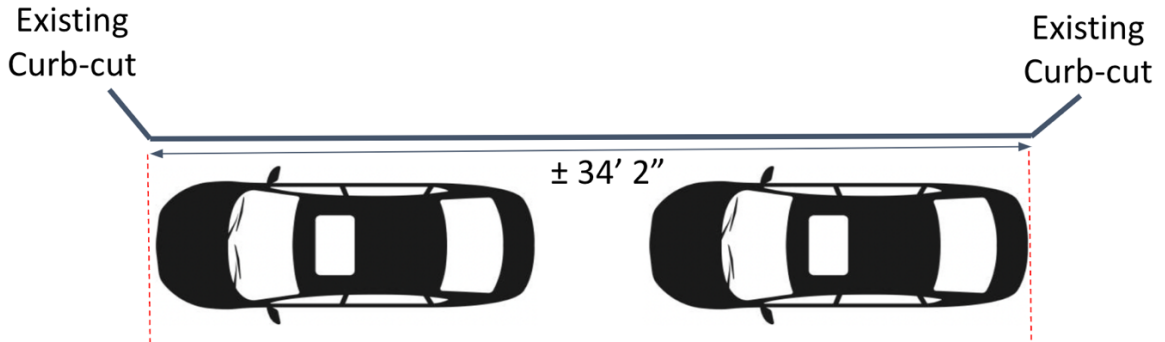
With a simple modification to their driveway, and a change to the length of their curb cut, from 10 foot to 9 foot, matching most of the houses on our street, the project sponsors could retain one parking spot right outside their own front door. (See exhibit 10)

To conclude, I would like to ask that the Board of Appeals reverse the decision made by DPW to issue permit 23IE-00435 with the curb cut location as proposed. I urge the Board to consider an alternative which would retain at least one 16' parking on the street.

# Exhibit 1

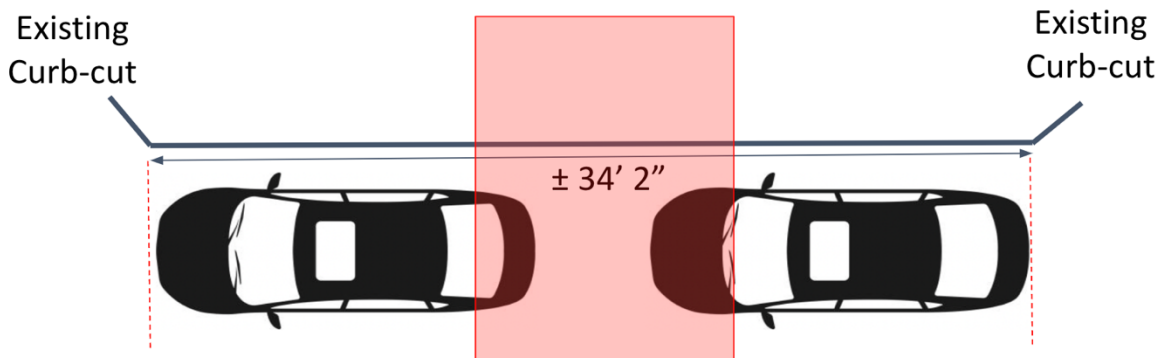
## Current Situation

Space sufficient for two cars  
(two 15' cars [Prius or similar] shown)

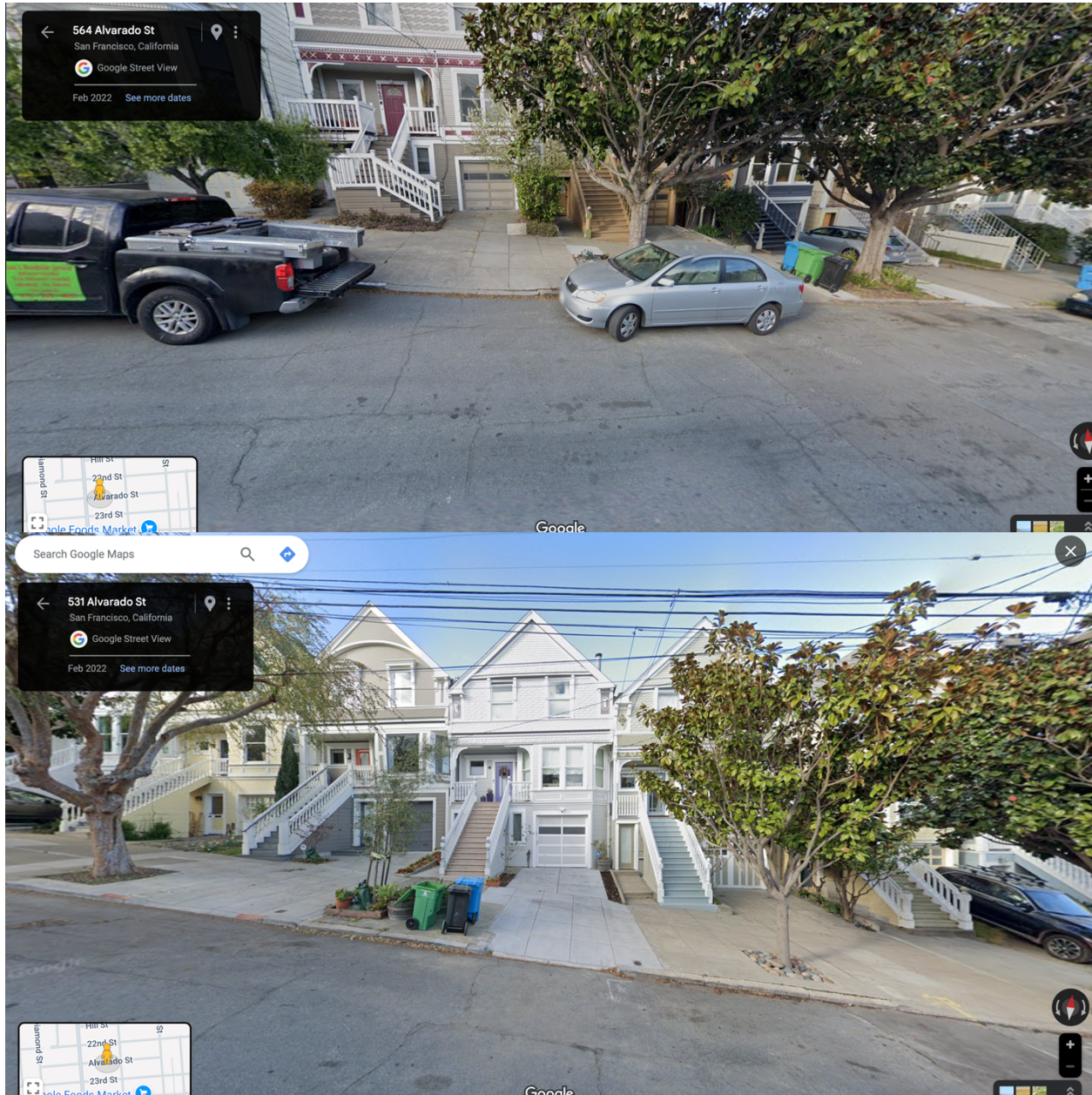


## Proposed Situation

With proposed curb-cut, only a microcar e.g. Smartcar would be able to make use of the public spaces



# Exhibit 2

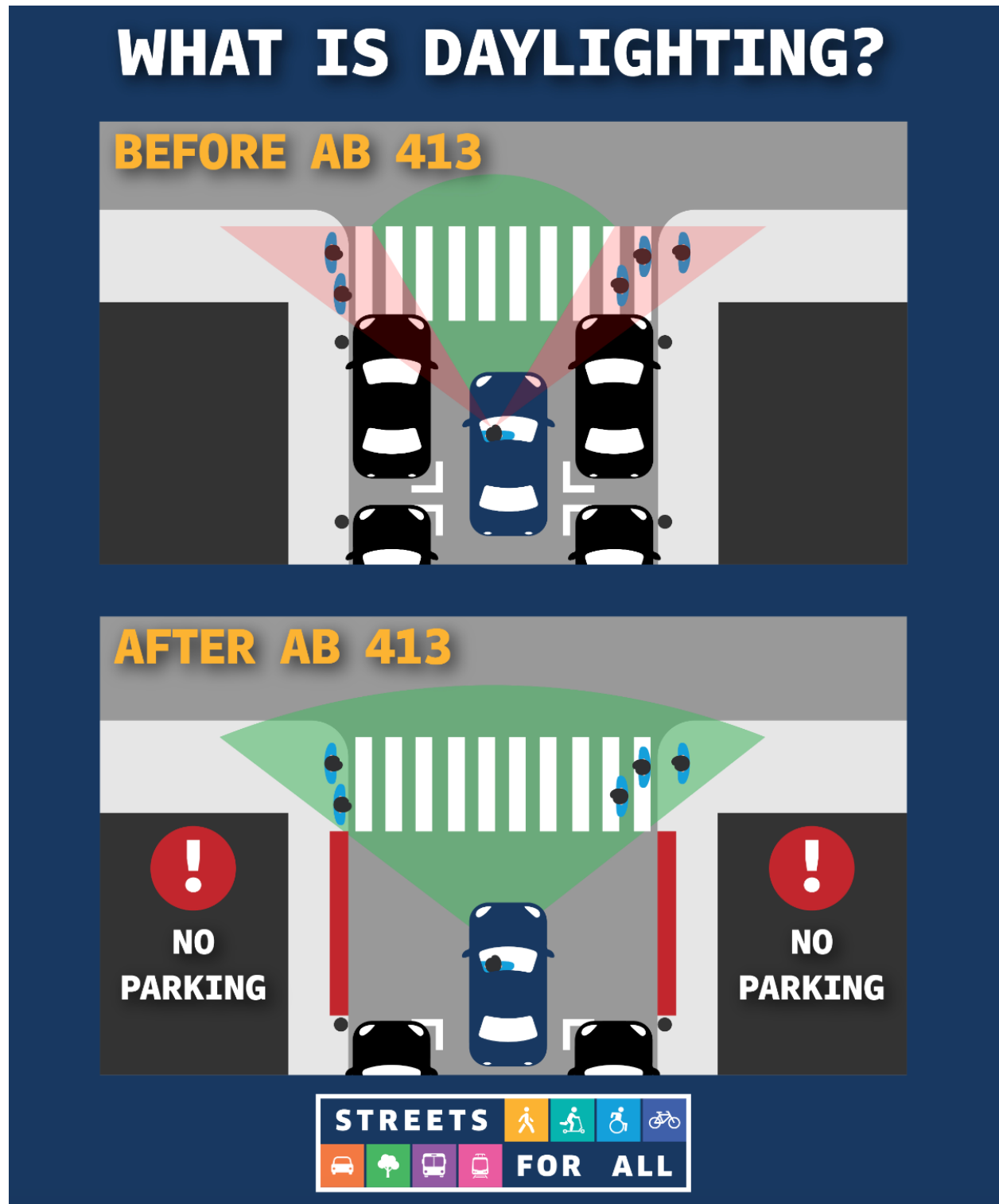






## Exhibit 3

AB 413 is sponsored by the transportation advocacy organization [Streets For All](#), and would prohibit vehicles from being stopped, left or parked within 20 feet of a marked crosswalk or intersection, a pedestrian safety measure known as “daylighting.”



## **Exhibit 4**



Catherine Woods &lt;cathywoods@gmail.com&gt;

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## Restoration of 551 Alvarado Street

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**Randall Laroche** <randall.laroche@sbcglobal.net>  
To: 500 Alvarado Street <500-alvarado@googlegroups.com>

Thu, Mar 7, 2024 at 2:12 PM

Dear Neighbors,

It has come to our attention that there is some misinformation that is being discussed about our renovation project at 551 Alvarado Street.

Since February 2023, we met with neighbors to review the plans and discussed this project in detail with Cathy. She raised some concerns about the design. While we did not need to, at costs to us, we subsequently made significant changes to the project in response to issues that she raised:

1. The pop-out addition at the rear was reduced in height by one story. This required reconfiguration of the stairs throughout the house, reconfiguration of rooms and an added stair run to the basement. This design change also resulted in more structural and foundation work.
2. Lowering the addition split up both dwelling units, so the bedroom for the lower unit was relocated to the basement and one of the bedrooms of the upper unit was relocated to the first floor. This created a more difficult fire-rating situation.
3. At the top floor, the vertical addition was setback 3' from the property line, per Cathy's request. This required that all of the rooms be reconfigured at the 3rd floor, and the loss of living space at that floor.
4. Lowering the addition at the rear also required a more complicated mechanical system to ventilate the crawl space due to less wall exposure at the rear.

We submitted our project March 2023, worked closely with Planning, Department of Building Inspections, Bureau of Urban Forestry, DPW, and all other required city agencies.

Our project will create two dwelling units with off street parking. There is a large deteriorating tree in front of the property and in order to accommodate off street parking, a curb cut will be made in front of the property; there will likely be a loss of **one** standard-size street parking space. (A standard on-street parking space is 20'-0" long.) The width of the proposed curb cut for the driveway is the minimum width it needs to be and was reviewed by SF Planning to meet the Zoning Administrator's Curb Cut policy and the residential design guidelines. Notwithstanding, most of the street has a similar driveway and off street parking.

We are committed to complying with all city regulations. The property, with the garage entrance, is consistent with nearly 85% of the homes on the block. We are not seeking an exception here.

This property has been vacant and in disrepair for many years with the homeless also utilizing the site. We hope to help create two needed residential homes and hope that we will continue to get support for this project as we complete our permitting process.

Sincerely,

Randall Laroche & David Laudon

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9/18/24, 8:19 PM

Gmail - Restoration of 551 Alvarado Street

You received this message because you are subscribed to the Google Groups "500 Alvarado Street" group.

To unsubscribe from this group and stop receiving emails from it, send an email to [500-alvarado+unsubscribe@googlegroups.com](mailto:500-alvarado+unsubscribe@googlegroups.com).

To view this discussion on the web visit <https://groups.google.com/d/msgid/500-alvarado/51de2f5c-924a-4e1d-9e8a-2c8dd3b39e30%40sbcglobal.net>.

## **Exhibit 5**



Catherine Woods &lt;cathywoods@gmail.com&gt;

---

**Restoration of 551 Alvarado Street**

---

**Randall Laroche** <randall.laroche@sbcglobal.net>  
To: 500-alvarado@googlegroups.com

Mon, Mar 11, 2024 at 11:33 AM

Thank-you Jan for reading the proposal compiled on our behalf by our expediter. The project would eliminate one space, not two as is being alleged, and would provide in exchange parking for the future inhabitants of the house, possibly ourselves. BTW: 85% of the homes on our block of Alvarado have garages, nearly all added later as nearly all structures there predate the automobile age.

I'd like to reiterate that we've been *most* conscientious with concerns for the block, which we truly admire and respect (this is the *only* street I'm aware of that has an e-mail communication list -- Bravo!) and have very generously made alterations in our original plan to appease a neighbor (as stated in the proposal), even though such was *not* required.

My spouse, David and I are *not* mere speculators trying to *make easy money* by doing nominal remodeling. I'm a 3rd generation San Franciscan with concern for retaining the city's historic homes by restoring/upgrading them; this will be the 4th such project that we've undertaken .. We've proof of past restorations which we'll happily share with anyone (some saw these at the last two *Block Parties*) and we'll gladly meet with anyone face-to-face should you wish to know further our plans.

500 Alvarado's a *fine* block and we feel honored to be improving it for all of us.

Randy Laroche and David Laudon

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[Quoted text hidden]

To view this discussion on the web visit <https://groups.google.com/d/msgid/500-alvarado/CA1DB9F9-5CCD-449E-9A15-841250441D44%40gmail.com>.

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**Exhibit 6**



Catherine Woods <cathywoods@gmail.com>

### Restoration of 551 Alvarado Street

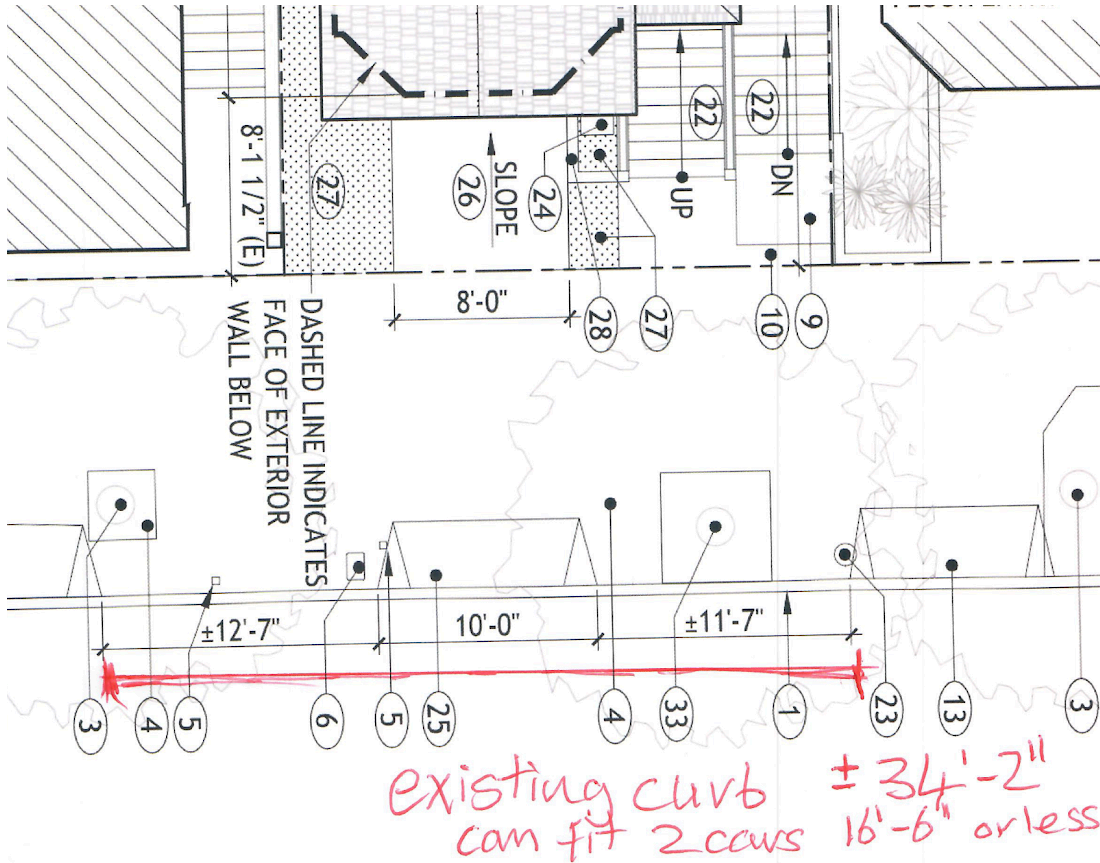
Catherine Woods <cathywoods@gmail.com>

Mon, Mar 11, 2024 at 12:27 PM

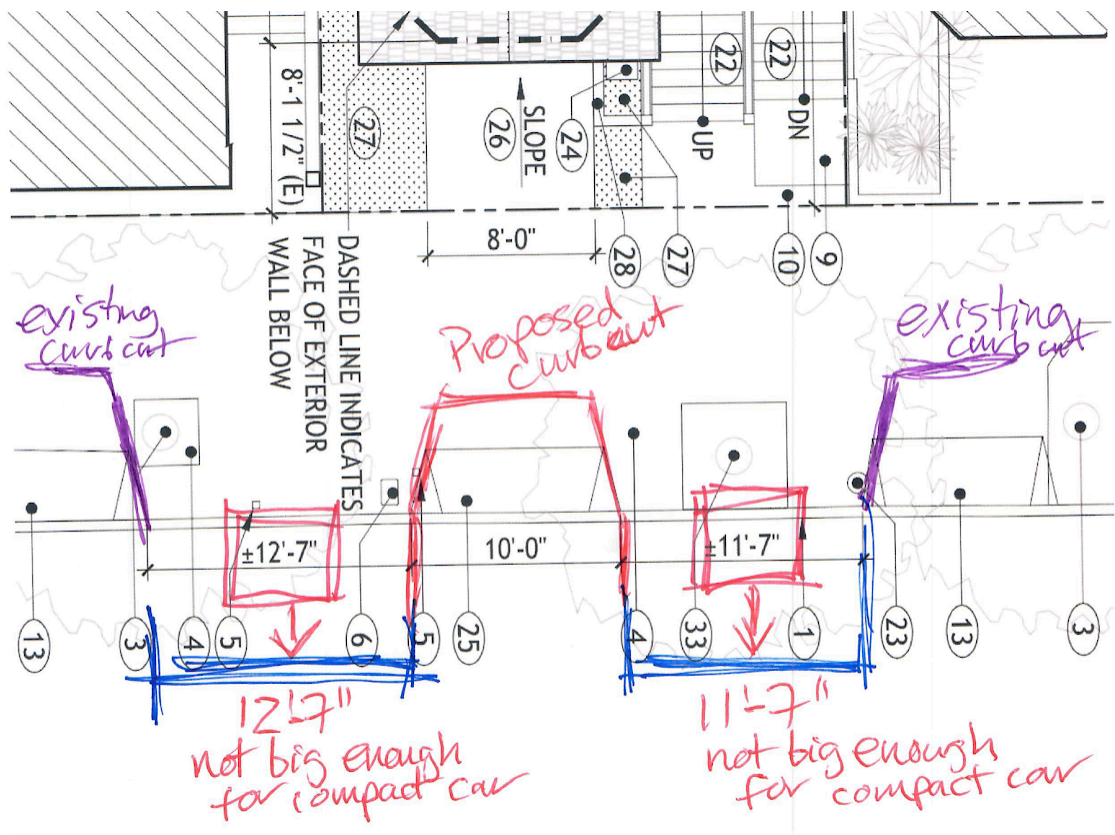
To: Randall Laroche <randall.laroche@sbcglobal.net>, "Gaime, Berhane (DPW)" <Berhane.Gaime@sfdpw.org>, "Rivera, Javier (DPW)" <Javier.Rivera@sfdpw.org>, "Steinberg, David (DPW)" <david.steinberg@sfdpw.org>

Randy & David,

Unless you have changed your plans, and please let us know if you did, they remain, as is, on the city website. Presently, there is approximately 34'-2" available for street parking outside 551 Alvarado St., which can fit 2 compact cars.



You are proposing to put a curb cut smack bang in the middle of that as shown below.



Please have your architect draw me a diagram showing me how I am incorrect.

FYI - AB 413 (Lee) - This new law prohibits the stopping, standing or parking of a vehicle within 20 feet of any crosswalk, whether it's marked or not, and within 15 feet of a crosswalk with a curb extension. - <https://www.sfmta.com/blog/new-state-laws-are-coming-streets-san-francisco>

We are set to lose 3-4 parking spots from the street, so parking is going to be a hot topic.

[Quoted text hidden]

**Exhibit 7**



Catherine Woods &lt;cathywoods@gmail.com&gt;

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## Curb cut at 551 Alvarado St renovation project

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**Sue Green** <suegreen42@gmail.com>  
To: Catherine Woods <cathywoods@gmail.com>

Wed, Sep 18, 2024 at 1:48 PM

Hi Catherine,

I really appreciate the clear diagrams illustrating the intent and effect that the proposed curb cut for 551 Alvarado will have on street parking. I also appreciate your continuing attempts to get clarity from the planning department despite a less than clear process.

It is self-evident that with the proposed curb cut the two parking spaces will only fit a motorbike or micro car OR two cars belonging to the owners/residents of 551 who can park over their driveway.

With the scope of the development it would seem likely that there will be at least 2 if not 3 new cars introduced onto the street.

This plan effectively removes 2 publicly available parking spaces from our street which will be in addition to the 3 that we will lose with new SF laws going into effect (no parking within 15' of a corner).

**A total of 5 fewer parking spaces on an already impacted block.**

Angling the curb cut would at least preserve one of these spaces for anyone on the block with a standard car to use. I hope that the planning department will at least consider the idea and not dismiss these concerns out of hand.

Kind regards,  
Sue

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[Quoted text hidden]

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Thanks,

Sue Green

(415) 309 0531



Catherine Woods &lt;cathywoods@gmail.com&gt;

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## Restoration of 551 Alvarado Street

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**Elizabeth Brogna** <ebrogna@sbcglobal.net>

Thu, Mar 7, 2024 at 5:59 PM

To: Catherine Woods &lt;cathywoods@gmail.com&gt;, Rachel Perahia &lt;rachel.perahia@gmail.com&gt;

Cc: Randall Laroche &lt;randall.laroche@sbcglobal.net&gt;, 500 Alvarado Street &lt;500-alvarado@googlegroups.com&gt;, "berhane.gaime@sfdpw.org" &lt;berhane.gaime@sfdpw.org&gt;, "javier.rivera@sfdpw.org" &lt;javier.rivera@sfdpw.org&gt;, "david.steinberg@sfdpw.org" &lt;david.steinberg@sfdpw.org&gt;, Michel Kapulica &lt;michel.kapulica@gmail.com&gt;

Hi All,

We also support Catherine's position on this. Spaces are limited on the block as it is and losing two spaces to gain a garage that will accommodate one car just doesn't make sense.

Elizabeth Brogna and Michel Kapulica  
526 Alvarado

[Quoted text hidden]

[Quoted text hidden]

[Quoted text hidden]

To view this discussion on the web visit <https://groups.google.com/d/msgid/500-alvarado/BCE4A75B-5AB8-460E-AE73-8E15F5D806C3%40gmail.com>.



Catherine Woods <cathywoods@gmail.com>

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## Permit issued for 551 Alvarado St, SF 94114

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**June Rose** <june\_rose@msn.com>  
To: Catherine Woods <cathywoods@gmail.com>

Mon, Mar 4, 2024 at 2:39 PM

Catherine,

Let me know where I can register my complaint.

June

[Quoted text hidden]

[Quoted text hidden]

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Catherine Woods &lt;cathywoods@gmail.com&gt;

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**Permit issued for 551 Alvarado St, SF 94114**

---

**Mark Schaaf** <mschaaf@gmail.com>

Tue, Mar 5, 2024 at 7:57 AM

Cc: Catherine Woods <cathywoods@gmail.com>, CPC-RecordRequest <CPC-RecordRequest@sfgov.org>, dbi.cpbrequest@sfgov.org, dbi.sunshinerequests@sfgov.org, dbi.records3R@sfgov.org, dbi.communications@sfgov.org, Thomas.Fessler@sfgov.org, subdivision.mapping@sfdpw.org, david.steinberg@sfdpw.org, Javier.Rivera@sfdpw.org, Berhane.Gaime@sfdpw.org, Raphael Haas <raph.haas@gmail.com>, Sue <suegreen42@gmail.com>

Hi,

Thanks, Catherine for your efforts here. I agree with this as well.

DBI Teams, please let me know where to file an additional complaint.

Thanks,  
Mark

[Quoted text hidden]

[Quoted text hidden]

To view this discussion on the web visit <https://groups.google.com/d/msgid/500-alvarado/3CBE52DE-DFCC-4DA8-8134-6164BEFF34D1%40gmail.com>.





Catherine Woods &lt;cathywoods@gmail.com&gt;

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## Restoration of 551 Alvarado Street

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**Jan Masaoka** <janmasaokag@gmail.com>

Sun, Mar 10, 2024 at 9:45 PM

To: Nick Orton &lt;nickorton86@gmail.com&gt;

Cc: Elizabeth Brogna &lt;ebrogna@sbcglobal.net&gt;, Alvarado block club &lt;500-alvarado@googlegroups.com&gt;, Catherine Woods &lt;cathywoods@gmail.com&gt;, Michel Kapulica &lt;michel.kapulica@gmail.com&gt;, Rachel Perahia &lt;rachel.perahia@gmail.com&gt;, Randall Laroche &lt;randall.laroche@sbcglobal.net&gt;, "berhane.gaime@sfdpw.org" &lt;berhane.gaime@sfdpw.org&gt;, "david.steinberg@sfdpw.org" &lt;david.steinberg@sfdpw.org&gt;, "javier.rivera@sfdpw.org" &lt;javier.rivera@sfdpw.org&gt;

I agree that if the building is improved and makes room for more people it's a good thing. We can stand the loss of one parking space. Jan/580

[Quoted text hidden]

[Quoted text hidden]

To view this discussion on the web visit <https://groups.google.com/d/msgid/500-alvarado/CABxg%3Dj1TzLJ98bMjBM56ZqLVOzMq45c7i0-Nn57p6PHF7U5uOA%40mail.gmail.com>.



Catherine Woods <cathywoods@gmail.com>

---

## Curb cut at 551 Alvarado St renovation project

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**Vesma Grinfelds** <dzvesma@gmail.com>  
To: Catherine Woods <cathywoods@gmail.com>

Mon, Sep 16, 2024 at 7:15 PM

Hi!

Is there a proposal on the table for an alternate way to create entry into the garage - like on a slant/angle so that there would be one spot left available? Leaving no other option really is not viable.

Vesma

[Quoted text hidden]

9/19/24, 9:39 AM

Gmail - Restoration of 551 Alvarado Street



Catherine Woods <cathywoods@gmail.com>

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## Restoration of 551 Alvarado Street

---

**Rachel Perahia** <rachel.perahia@gmail.com>

Thu, Mar 7, 2024 at 5:43 PM

To: Catherine Woods <cathywoods@gmail.com>

Cc: Randall Laroche <randall.laroche@sbcglobal.net>, 500 Alvarado Street <500-alvarado@googlegroups.com>, Berhane.Gaime@sfdpw.org, Javier.Rivera@sfdpw.org, david.steinberg@sfdpw.org

I want to chime in to support Catherine's position. Street parking is already limited on our block. The proposed approach will negatively impact everyone by removing two spots in order to add one very small garage that may not be used given its size. We would welcome information from the city on what process we need to follow to make an objection. Could the DPW officials on this email please let us know how to do that?

Thanks,  
Samir and Rachel Gupte  
561 Alvarado

## **Exhibit 8**

### **PLACEMENT OF THE GARAGE AND CURB CUT**

The location of the curb cut, garage, and garage door should ensure maximum compatibility with existing on-street parking, existing dwelling units, and the structure's context. Greater numbers of entryways and units along a building activate more of the street frontage by increasing the points where residents come and go as well as the number of opportunities for personalization.

- On-Street Parking. Could a greater number of on-street parking spaces be retained if the curb cut and garage were shifted elsewhere on the building's frontage?

**Exhibit 9**



SAN FRANCISCO  
**PLANNING**  
DEPARTMENT

# Guidelines for **Adding Garages and Curb Cuts**

CRITERIA FOR ADDING GARAGES  
AND CURB CUTS TO EXISTING STRUCTURES  
( INCLUDING HISTORIC RESOURCES )

Formerly known as: Zoning Administrator Bulletin Nos. 2006.1a and 2006.1b





**ORGANIZATION:**

This document is divided into two sections, both describe the quantitative and qualitative measures used to review proposed projects.

**General Standards and Criteria for Existing Buildings**

**Additional Standards and Criteria for Known and Potential Historic Resources**

This bulletin explains Planning Department procedures for the review of building permit applications proposing to add off-street parking to existing residential structures. The review requirements of other City agencies, such as the Department of Building Inspection (DBI) or the Department of Public Works (DPW), are not addressed in this document.

Approval of such building permit applications may be granted at the Planning Information Counter (PIC). In other instances, the application may be routed to a planner for further staff review, after which it may be approved, modified, or disapproved.

It is strongly recommended that all applications be reviewed first at the PIC prior to finalizing any plans. An initial determination can be made by PIC Staff as to whether the subject structure is a “historic resource” or a “potential historic resource.”

Rehabilitation and alteration standards for the preservation of designated City Landmark properties are contained in Article 10 of the Planning Code. However, there are structures within San Francisco that are considered “historic resources” in addition to Landmark properties.

For the purposes of this bulletin these structures or “historic resources” are buildings constructed in or before 1913 that appear to be of historic or architectural merit and those previously evaluated and included on specified registers and surveys. This also includes properties over fifty years of age that may be found to be historic resources based on available historic information.

Any proposal to add a new garage in a structure that is considered a known or potential historic resource is subject to the additional requirements outlined within this bulletin.

Inserting a new garage opening can have a major impact on a historic building and the surrounding neighborhood. Due to this potential impact, the Planning Department reviews proposals for new garages on a case-by-case basis. Department staff will review all proposals for compatibility with the Secretary of the Interior’s Standards for the Treatment of Historic Properties. These Standards were developed by the National Park Service and are applied as set forth by the California Environmental Quality Act (CEQA) Guidelines Section 15331. **It is important to note that as legal non-conforming structures, the Planning Code does not require the provision of off-street parking for these properties.**

For all other structures, Planning Staff at the PIC will determine compliance with the Planning Code, the General Plan, the Residential Design Standards<sup>1</sup> and the specific criteria contained below. Should the proposed curb cut and garage door meet these standards, the application may be approved at the PIC.

# General Standards and Criteria for Existing Buildings

## GARAGE DOOR APPEARANCE

Garage door design and materials should be compatible with the existing building and surrounding neighborhood character.

## PLACEMENT OF THE GARAGE AND CURB CUT

The location of the curb cut, garage, and garage door should ensure maximum compatibility with existing on-street parking, existing dwelling units, and the structure's context. Greater numbers of entryways and units along a building activate more of the street frontage by increasing the points where residents come and go as well as the number of opportunities for personalization.

- On-Street Parking. Could a greater number of on-street parking spaces be retained if the curb cut and garage were shifted elsewhere on the building's frontage?
- Impact to Existing Dwelling Units. Would the proposed placement of the new garage have a negative effect on any existing dwelling units on the ground level?
- Loss of Existing Street Trees.<sup>2</sup> Could existing street trees adjacent to the subject property remain if the garage and/or curb cut were shifted elsewhere on the building's frontage?
- Loss of existing Significant Trees.<sup>3</sup> Could existing Significant Trees within the subject parcel remain if the garage and/or curb cut were shifted elsewhere on the building's frontage?<sup>4</sup>



Front yard setbacks that not only enliven the public realm but also represent the historic pattern of development should be maintained and protected. When a garage is necessary, it should be inserted into the building, avoiding impacts on the character-defining features of the building and the displacement of any ground floor residential units.



## WIDTH OF THE GARAGE DOOR AND CURB CUT

The total width of the garage door should be no larger than necessary to accommodate the off-street parking space. The total width of the curb cut should not exceed the Planning Department's standard curb cut maximum of 10 feet.<sup>5</sup>

In any instance where a proposed curb cut or garage door exceeds either of these dimensional requirements, the application will be routed upstairs for further review, where the burden will be upon the applicant to show that there are special circumstances that warrant larger dimensions, such as:

- Site Constraints. Is there a severe (1) lateral slope or (2) grade change in the front setback? Is the width of the lot or sidewalk atypical?
- Street Constraints. Is the width of the street prohibitively narrow such that maneuvering a standard automobile into the proposed garage is not possible?
- Limited Garage or Building Depth. When proposing a new multiple-space garage, could the garage be made deeper?
- On-street Parking Spaces. Does excessive curb cut or garage door width further decrease the number of available on-street parking spaces?
- Over-parking. Would the proposal result in the provision of more than one parking space per dwelling unit?

## STREET TREES

Are new street trees included in the proposal? If existing street trees would be removed, are replacement trees of similar caliper and canopy size proposed?



The base of this historic projecting bay was adversely impacted for this garage. This treatment does not meet the criteria for inserting a garage within an existing structure.

## BUILDING EXPANSION

Would any exterior dimension of the structure be increased? If building must be lifted to accommodate the garage, or if the proposal involves an exterior expansion, neighborhood notification may be required and the building permit application cannot be approved at the PIC. The application will be routed to a Planner for further review.

## INTERFERE WITH TRANSIT, BICYCLES, OR PEDESTRIANS

New or expanded garages or curb cuts that are located along Transit Preferential Streets or that would otherwise adversely transit stops, bicycle routes, or primary pedestrian streets cannot be approved over the counter. The application will be routed to a planner for further review.

ILLUSTRATIVE EXAMPLES

RIGHT: The property is not a good candidate for the insertion of a garage. The base of the building is short and constructing a garage would require the removal of character-defining features. Raising the building would significantly change its height and would adversely impact its overall design and its relationship to the street and its immediate neighbors.



ABOVE: This garage structure acts as a barrier between the residential building and the public realm, degrading the pedestrian experience. The construction of a garage structure within the front yard setback has not only removed historic materials at the base of this building, it has also resulted in the alteration of the historic stair configuration.

LEFT: This garage meets the criteria. The garage opening does not adversely impact the projecting bay above and has been designed to be the minimum width necessary in order to reduce the removal of historic material while maximizing landscaping within the front setback.

# Additional Standards and Criteria for Historic Resources

The ongoing demand for off-street parking in San Francisco has created a serious challenge for its historic resources. This bulletin is written to ensure adequate and consistent review of the City's known and potential historic resources.

## CHARACTER-DEFINING FEATURES

Below is a list of the character-defining features that, if altered, may trigger additional Planning Department review. Please note that in some instances the insertion of a garage opening in a historic resource will not be approved.

### Architecture:

- Bays
- Decorative features
- Front entries

### Relationship to adjacent buildings and streetscape:

- Significant trees<sup>6</sup>
- Historic fences
- Historic pattern of development

### Massing & Scale:

- Height
- Front Setbacks

## RAISING STRUCTURES

Generally, raising a historic resource to insert a garage opening is strongly discouraged when the act may render the building ineligible for the California or National Register. In some instances, raising a structure to insert a garage opening may be approved to avoid the removal of historic fabric as long as the integrity of the building and its original design, proportions, and relationship to adjacent buildings are not compromised.

## ADDITIONAL CRITERIA

In cases where a garage opening may be appropriate, great care should be taken in the design and execution of the work. In addition to the criteria set forth in this document, the following criteria apply to the review of new garages in historic resources.

- A garage openings should be inserted on the side or rear whenever possible. These "secondary elevations" have fewer character-defining features.
- A new opening and curb cut should be no larger than absolutely necessary while still meeting the requirements of the existing Building and Planning Codes.
- All detailing, including garage doors, surrounds, and decorative features, should be compatible with the building's architectural features without creating a false sense of history.
- To avoid impacts to character-defining features, the project sponsor may explore obtaining a Minor Encroachment Permit (Section 723.2 of the Public Works Code) from the Department of Public Works (DPW). This permit allows for the extension of the driveway into the public right-of-way and can lower the height of the garage door to avoid the removal of character-defining features. DPW can be reached at (415) 554-5810.
- Garages should be designed to be inconspicuous so they do not project out from the front façade of the building; however, new garage structures in the front yard setback of steeply sloping lots or in retaining walls may be appropriate.
- Landscape improvements should be incorporated into the proposal to minimize the impact a new garage opening has on the building and the surrounding streetscape.

ILLUSTRATIVE EXAMPLES



ABOVE LEFT: To maximize landscaping within narrow front setbacks, consider a "Hollywood" driveway, as depicted above, or an open cell paver to allow for grass to grow through and to minimize stormwater runoff.

ABOVE RIGHT: The insertion of two separate garage openings that run the entire width of the building is not recommended because it erodes the public realm. Additionally, the width of the curb cut removes more street parking than necessary.

RIGHT: Historic fences, should be taken into consideration when proposing a new garage opening and should be salvaged and reinstalled as appropriate to preserve these rare historic features.



## NOTES

- 1 The Residential Design Standards (formerly Residential Design Guidelines, December 2003) contain recommended standards for new garage openings and curb cuts on pages 34-36.
- 2 Should any street tree removal be proposed, the application would be routed upstairs for further review while a Street Tree Removal Permit is sought from the Urban Forestry Division of the Department of Public Works (DPW).
- 3 For purposes of this Bulletin, a Significant Tree is defined in Public Works Code Section 810A as a tree within 10 feet of the front property line which meets at least one of the following criteria: (a) a diameter at breast height (DBH) in excess of 12 inches, (b) a height in excess of 20 feet, or (c) a canopy in excess of 15 feet. Any removal of or impact to Significant Trees would result in the application being routed upstairs for further review while the applicant pursues required permits from DPW.
- 4 This is a restatement of policies set forth in Zoning Administrator Bulletin 2. While ZA Bulletin 2 presents background information and establishes a policy foundation for the regulation of curb cuts, these guidelines expand on and supersede the policies contained in ZA Bulletin 2.
- 5 The Department's standard curb cut (7 feet across at the street level and 18 inch transition slopes [where the curb tapers down to the street] on either side) was established in Zoning Administrator Bulletin 2 and is reiterated on page 37 of the Residential Design Standards (formerly Residential Design Guidelines, December 2003). This is a restatement of policies set forth in Zoning Administrator Bulletin 2. While ZA Bulletin 2 presents background information and establishes a policy foundation for the regulation of curb cuts, this bulletin expands on and supersedes the policies contained in ZA Bulletin 2.
- 6 See footnote 3.

Cover photo by Jaymi Heimbuch  
<http://www.flickr.com/photos/jaymiheimbuch/4446078093>



**SAN FRANCISCO  
PLANNING  
DEPARTMENT**

### FOR MORE INFORMATION: Call or visit the San Francisco Planning Department

**Central Reception**  
1650 Mission Street, Suite 400  
San Francisco CA 94103-2479

TEL: **415.558.6378**  
FAX: **415 558-6409**  
WEB: **<http://www.sfplanning.org>**

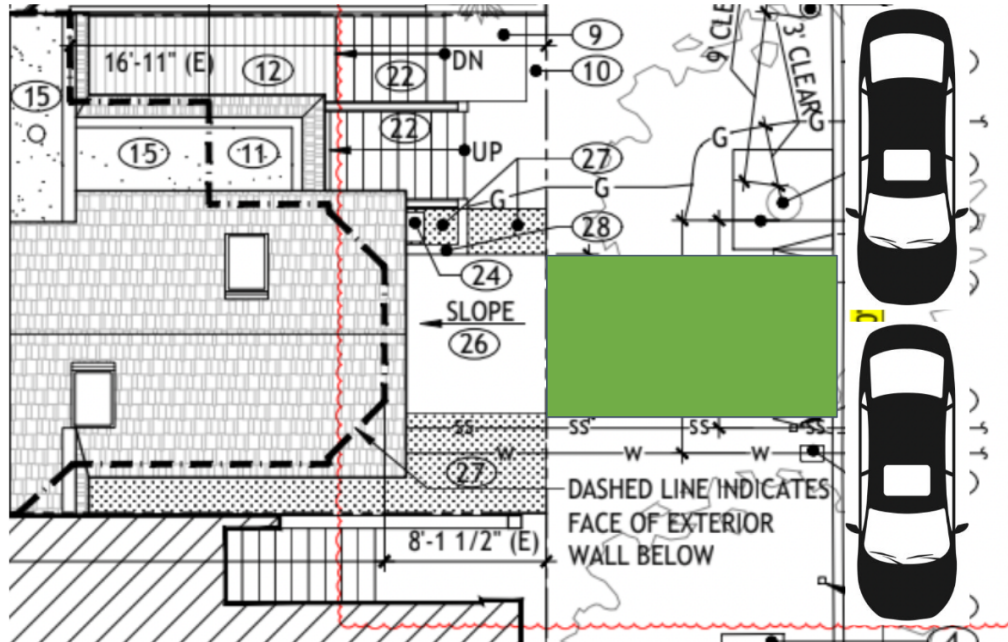
**Planning Information Center (PIC)**  
1660 Mission Street, First Floor  
San Francisco CA 94103-2479

TEL: **415.558.6377**

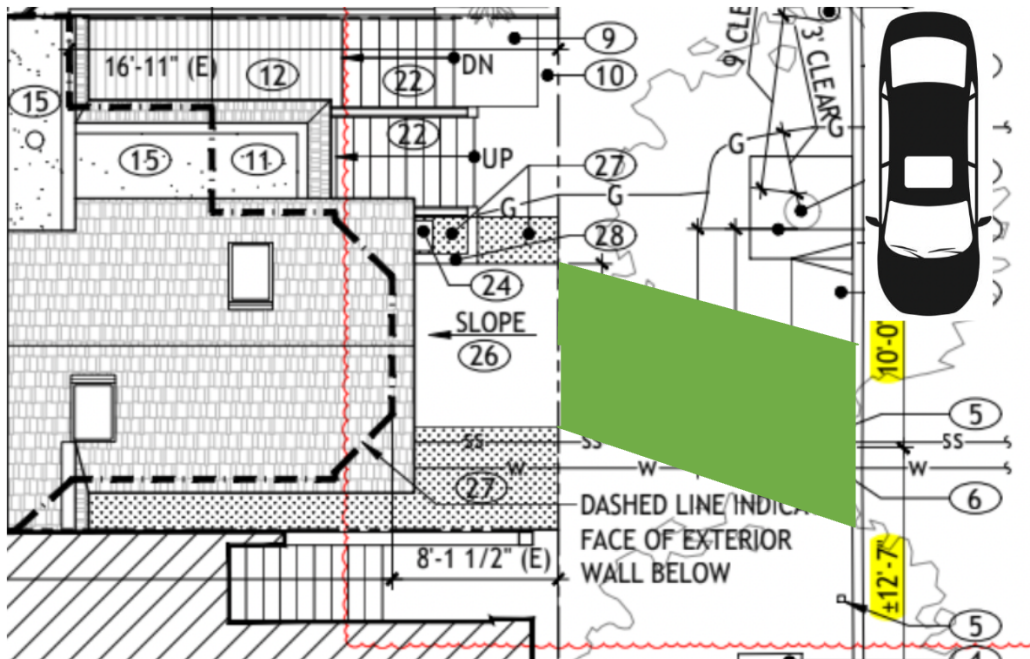
*Planning staff are available by phone and at the PIC counter.  
No appointment is necessary.*

# Exhibit 10

## Proposed Situation



## Possible Solution - leaving 1 spot on the street



## **BRIEF SUBMITTED BY THE PERMIT HOLDER(S)**



October 3, 2024

Delivered Via Email ([boardofappeals@sfgov.org](mailto:boardofappeals@sfgov.org))

San Francisco Board of Appeals  
49 S Van Ness Ave Suite 1475  
San Francisco, CA 94103

Re: 551 Alvarado – (No. 24-050)  
Permit No.: 23IE-00435

Dear Board of Appeals:

This brief is submitted on behalf of the permit holders, Randall Laroche and David Laudon (“project sponsors”), in response to the appeal filed by Catherine Woods concerning the Street Improvement Permit (23IE-00435) issued for the two-family dwelling located at 551 Alvarado Street (the “Project”).

## **A. Background**

The project sponsors purchased an uninhabitable, vacant single family dwelling unit with an illegal and unauthorized unit July 2022. They worked thoughtfully and with neighbors to design and develop two legal units, of which one is family sized. The proposed curb cut and related improvements have undergone comprehensive reviews by multiple city agencies over the past 1.5 years.

The curb cut was included in the initial design and had not changed from what was presented at the neighborhood Pre-application meeting (*see Attachment 1*), submitted and approved by Planning (*see Attachment 2*), submitted and approved by the Building Department under Building Permit 202303224205 (*see Attachment 2*). It was also noted in the application for the Tree Removal Permit as well. (*see Attachment 3*).



**B. Current Placement of Curb Cut is safest and no other alternative is possible:**

1. Project sponsor hired Lea & Braze Engineering to conduct an analysis to determine the location of the curb cut. In order for a vehicle to safely enter and exit the garage, the driveway cut needs to align with the garage door as much as possible. Any other alternative location of the curb cut would make it unsafe for the driver, pedestrians and other nearby vehicles as there would be no clearance at the sides of the vehicle when entering the garage. Additionally, in order to exit out of the garage, the vehicle will need to make a sharp turn that overlaps the entrance of the house; there would simply be insufficient space for the vehicle to turn in anyway to make it through the curb cut. (*see Attachment 4*).
2. Current location of curb cut is code compliant and underwent multi agency review and approvals. The placement of the driveway is consistent and follows the pattern of all the homes on the block that have a garage except for Appellant's property. 551 Alvarado's driveway is compliant with the Residential Design Guidelines. The curb cut location and related improvements were reviewed and approved by both the Planning Department and DPW. All necessary steps were followed, including the required public notifications, reviews, and environmental clearances.
3. The remaining curb would allow for motorcycle parking, Mini Coopers, Mirages, etc. It would also provide additional needed clearance for Appellant's entrance to her garage.

**C. Project Sponsor worked in good faith** with Appellant. Appellant provided us her concerns in writing. (*see Attachment 5*). In response to her concerns, with detriment to the design and at extra costs, many changes were made to be responsive to her issues. (*see Attachment 6*). Any last-minute changes to this project would increase the cost significantly and would require suspending construction given the availability of construction funds.

## **D. Conclusion**

Project sponsors thoughtfully designed a code compliant project and committed significant funds to renovate this property for their use and for providing additional housing to SF. Careful balance between the need for off-street parking for the family sized unit and the City's transit first policies was considered. For the reasons stated above, we respectfully request that the Board deny appellant's request and allow the project to proceed as planned. The Street Improvement Permit (see Attachment 6) was properly reviewed and issued following all regulatory procedures.

Sincerely,

A handwritten signature in black ink, appearing to read "Amy Lee". The signature is fluid and cursive, with a large initial "A" and "L".

Amy Lee, 3S, on behalf of  
David Laudon and Randall Laroche, Project Sponsor

Attachment 1 Pre-Application Meeting Plans

Attachment 2 Approved Site Permit Plans

Attachment 3 BUF permit and approved Plans

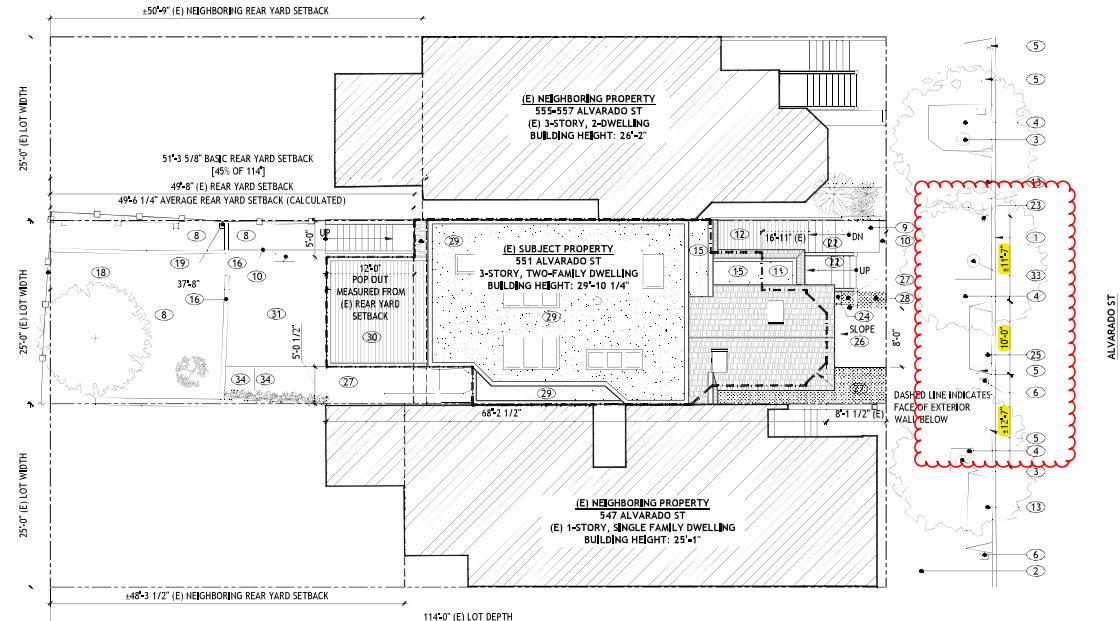
Attachment 4 Civil Engineer Email

Attachment 5 Design changes in response to Ms. Woods concerns

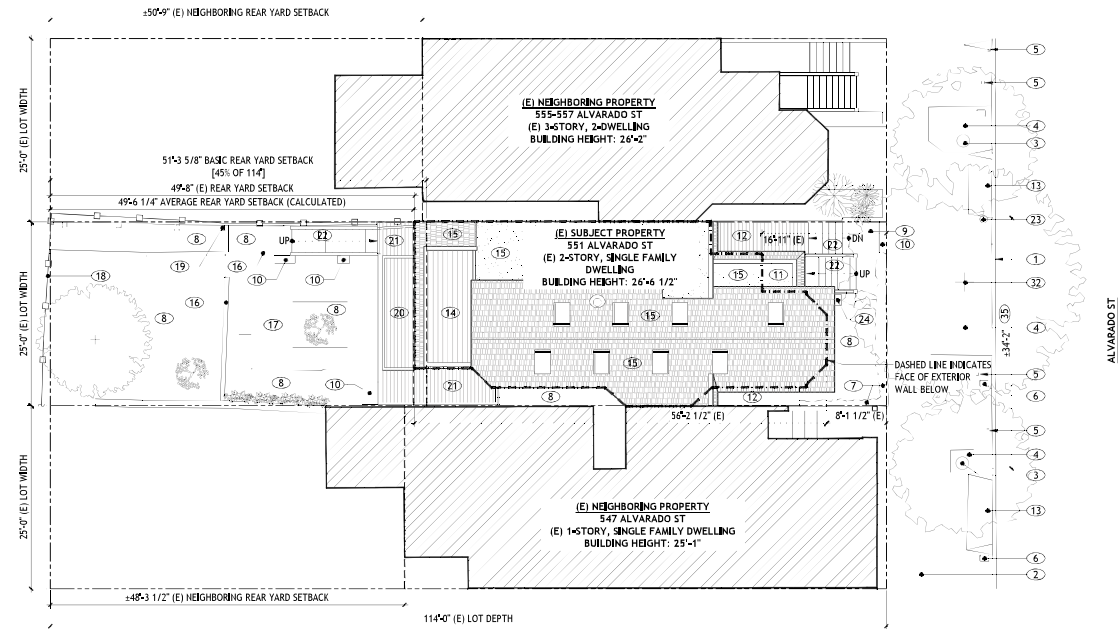
Attachment 6 Street Improvement Permit



VINCENT GONZAGA ARCHITECT  
 SAN FRANCISCO  
 415 690 7738  
 vincent@vgonzaga.com  
 1190 PINE ST APT 103  
 SAN FRANCISCO CA 94109



2 PROPOSED SITE PLAN  
 SCALE: 1/8" = 1'-0"



1 EXISTING SITE PLAN  
 SCALE: 1/8" = 1'-0"

KEYED NOTES

- ① (E) CURB
- ② (E) SIDEWALK
- ③ (E) STREET TREE
- ④ (E) STREET TREE PLANTER BOX
- ⑤ (E) SEWER CLEANOUT
- ⑥ (E) WATER METER
- ⑦ (E) STONE & CONCRETE FENCE
- ⑧ (E) LANDSCAPED AREA
- ⑨ (E) BRICK WALKWAY
- ⑩ (E) CONCRETE STEP
- ⑪ (E) COVERED PORCH BELOW
- ⑫ (E) WOOD WALKWAY
- ⑬ (E) CURB CUT
- ⑭ (E) DECK @ ATTIC/THIRD FLOOR
- ⑮ (E) ROOF
- ⑯ (E) CMU RETAINING WALL
- ⑰ (E) CONCRETE PATIO
- ⑱ (E) WOOD FENCE
- ⑲ (E) WOOD RETAINING WALL
- ⑳ (E) DECK @ SECOND FLOOR
- ㉑ (E) DECK @ GROUND FLOOR
- ㉒ (E) WOOD STAIRS
- ㉓ (E) WOOD STAIRS
- ㉔ (E) LIGHT/POWER POLE
- ㉕ (E) GAS METER
- ㉖ (N) CURB CUT
- ㉗ (N) DRIVEWAY
- ㉘ (N) LANDSCAPED (UNPAVED) AREA
- ㉙ (N) CONCRETE CURB
- ㉚ (N) ROOF
- ㉛ (N) ROOF DECK ABOVE SECOND FLOOR
- ㉜ (N) PAVED PATIO
- ㉝ (N) REMOVE (E) STREET TREE
- ㉞ (N) STREET TREE IN (N) 5' X 5' TREE BASIN
- ㉟ (N) COMPRESSORS FOR (N) HEAT PUMP SYSTEM
- ㊱ (N) STREET PARKING SPACE

LANDSCAPE AND PERMEABILITY

TYPE	EXISTING	PROPOSED
FRONT SETBACK AREA	203 SF	203 SF
LANDSCAPED AREA	91 SF	96 SF
LANDSCAPED PERCENTAGE	45%	28%
PERMEABLE AREA	123 SF	156 SF
PERMEABLE PERCENTAGE	61%	77%

PRE-APPLICATION MEETING 02/14/23  
 REV# DESCRIPTION DATE  
 ISSUE DATES / REVISIONS  
 PROJECT NAME

551 ALVARADO ST  
 ADDITION  
 551 ALVARADO ST  
 SAN FRANCISCO, CA 94114  
 BLOCK/LOT: 3624/039

PROJECT NUMBER 2214  
 SHEET TITLE

EXISTING AND PROPOSED  
 SITE PLANS

SHEET NUMBER

A001





City and County of San Francisco

San Francisco Public Works · Bureau of Urban Forestry

49 South Van Ness Ave, Suite 1000 • San Francisco, CA 94103

[sfpublishworks.org](http://sfpublishworks.org) · tel 628-652-8733

## **PERMIT FOR PLANTING, MAINTENANCE OR REMOVAL OF SIDEWALK TREES**

Pursuant to the provisions of Article 16, Sec. 806 of the Public Works Code, as amended, permission is hereby granted to:

Randall Laroche  
548 Castro St # PMB 516  
San Francisco CA 94114

**This permit specifically grants permission for:**

***Permit Remove Tree - 1***

**on the sidewalk adjacent to the premises at:**

***551 Alvarado St***

Permittee agrees to hold harmless the City and County of San Francisco, its officers, agents and employees from any damage or injury caused by reason of the planting or the placement or maintenance of the planter plants. The owner or owners of the respective property shall be solely liable for any damage. Per Article 16: Section 806.5.(b).(1) all work associated with a street tree permit must be completed within six (6) months of issuance, unless an extension has been granted by the Department. This permit must be on site when permission has been granted for removal of a street tree. If a tree species is not specified above, please contact Bureau of Urban Forestry to discuss an appropriate and approved species to plant.

**Special Conditions: Recommendation: After consideration of correspondence and testimony provided, the recommendation is to approve removal with replacement. As per Article 16 Section 806 tree replacement is required if infrastructure allows. Please plant using 3" depth of mulch as a top layer and water 25 gallons a week for the first 3-5 years to establish tree. New tree(s) approved for planting or replacement must be maintained (watered, weeded, restaked, etc.) for a period of three years at the end of which time the Department will conduct a follow-up inspection in order to assume all maintenance responsibilities. If the tree dies within the three-year maintenance period, the tree must be replaced at the property owner's expense and the maintenance period will restart. An inspection of the tree(s) by Urban Forestry is required before the city assumes responsibility. Tree must be established. Please contact our office at (628) 652-8733 or email [urbanforestrypermits@sfdpw.org](mailto:urbanforestrypermits@sfdpw.org) to schedule an inspection appointment or for any advice or assistance with watering, staking, mulching, or pruning.**

Date Issued: 6/10/2024

Permit No: 794772

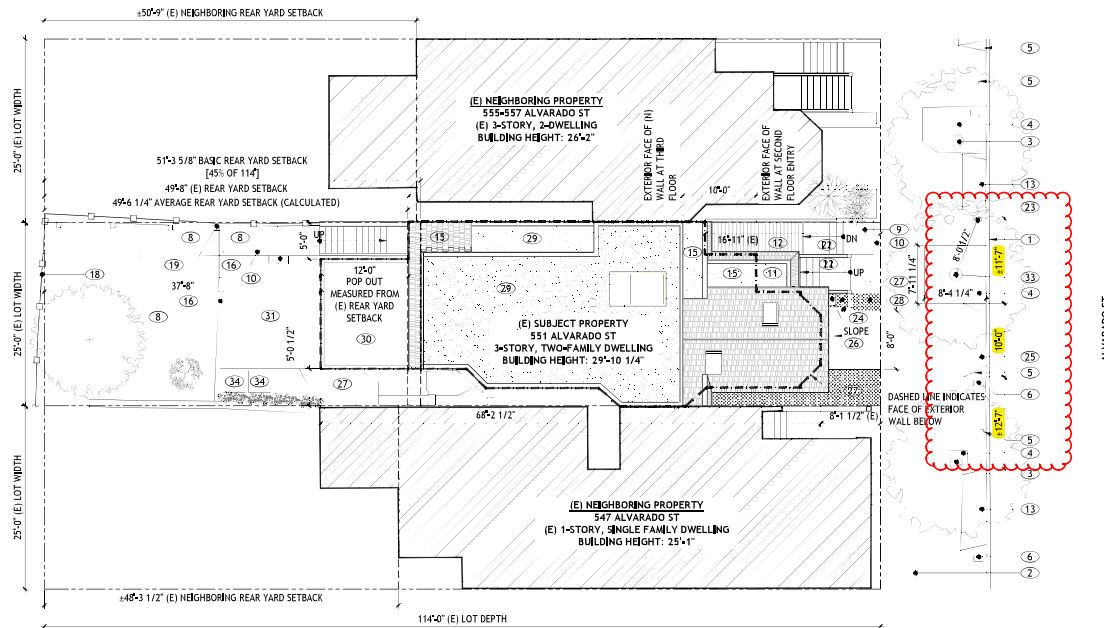
Approved:

Director, Department of Public Works

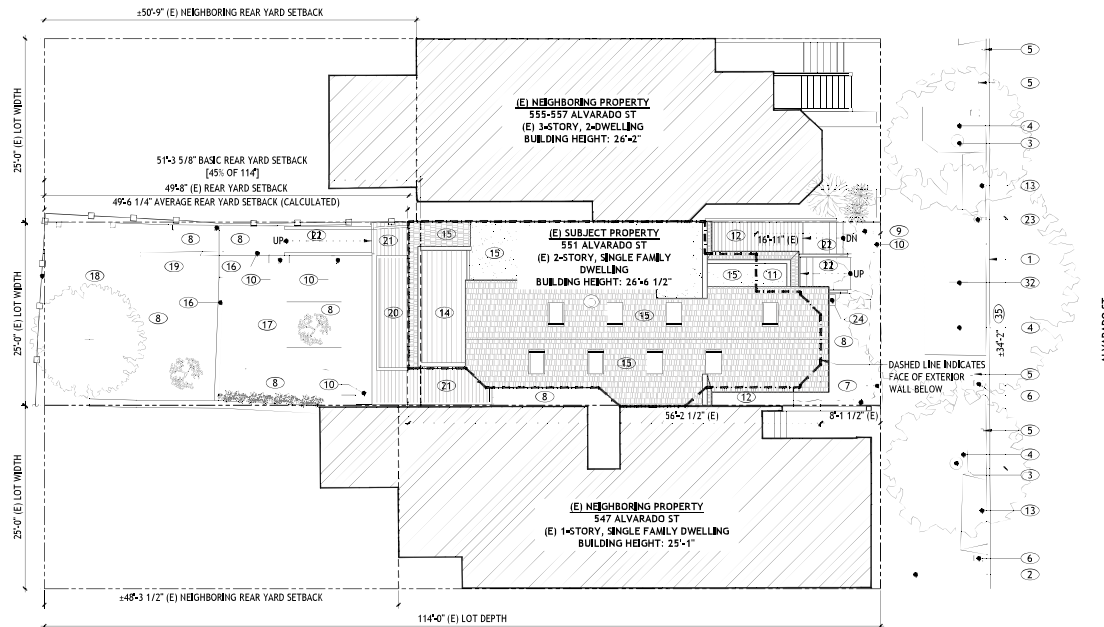
Handwritten signature of Chris Buck in cursive.

Chris Buck, Urban Forester

Bureau of Urban Forestry



2 PROPOSED SITE PLAN  
SCALE: 1/8" = 1'-0"



1 EXISTING SITE PLAN  
SCALE: 1/8" = 1'-0"

KEYED NOTES

- ① (E) CURB
- ② (E) SIDEWALK
- ③ (E) STREET TREE
- ④ (E) STREET TREE PLANTER BOX
- ⑤ (E) SEWER CLEANOUT
- ⑥ (E) WATER METER
- ⑦ (E) STONE & CONCRETE FENCE
- ⑧ (E) LANDSCAPED AREA
- ⑨ (E) BRICK WALKWAY
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- ⑪ (E) COVERED PORCH BELOW
- ⑫ (E) WOOD WALKWAY
- ⑬ (E) CURB CUT
- ⑭ (E) DECK @ ATTIC/THIRD FLOOR
- ⑮ (E) ROOF
- ⑯ (E) CMU RETAINING WALL
- ⑰ (E) CONCRETE PATIO
- ⑱ (E) WOOD FENCE
- ⑲ (E) WOOD RETAINING WALL
- ⑳ (E) DECK @ SECOND FLOOR
- ㉑ (E) DECK @ GROUND FLOOR
- ㉒ (E) WOOD STAIRS
- ㉓ (E) WOOD STAIRS
- ㉔ (E) LIGHT/POWER POLE
- ㉕ (E) GAS METER
- ㉖ (N) CURB CUT
- ㉗ (N) DRIVEWAY
- ㉘ (N) LANDSCAPED (UNPAVED) AREA
- ㉙ (N) CONCRETE CURB
- ㉚ (N) ROOF
- ㉛ (N) ROOF DECK ABOVE GROUND FLOOR
- ㉜ (N) PAVED PATIO
- ㉝ (N) REMOVE (E) STREET TREE
- ㉞ (N) STREET TREE IN (N) 5' X 5' TREE BASIN
- ㉟ (N) COMPRESSORS FOR (N) HEAT PUMP SYSTEM
- ㊱ (N) STREET PARKING SPACE

PLAN NOTES

1. SEE EXISTING AND PROPOSED ROOF PLANS ON A104 FOR ADDITIONAL INFORMATION.
2. SEE CIVIL DRAWINGS FOR ADDITIONAL INFORMATION.

LANDSCAPE AND PERMEABILITY

TYPE	EXISTING	PROPOSED
FRONT SETBACK AREA	203 SF	203 SF
LANDSCAPED AREA	91 SF	96 SF
LANDSCAPED PERCENTAGE	45%	28%
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PERMEABLE PERCENTAGE	61%	77%



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SAN FRANCISCO  
415 690 7738

vincent@vgonzaga.com  
1390 MARKET ST STE 200  
SAN FRANCISCO CA 94102



REV#	DESCRIPTION	DATE
1	TREE REMOVAL APP	08/23/23
2	COMMENT RESPONSE 3	08/07/23
3	COMMENT RESPONSE 2	06/14/23
4	COMMENT RESPONSE	04/27/23
	SITE PERMIT	03/13/23
	PRE-APPLICATION MEETING	02/14/23

PROJECT NAME  
**551 ALVARADO ST ADDITION**  
551 ALVARADO ST  
SAN FRANCISCO, CA 94114  
BLOCK/LOT: 3624/039

PROJECT NUMBER 2214  
SHEET TITLE

EXISTING AND PROPOSED SITE PLANS

SHEET NUMBER

**A001**



## Re: 551 Alvarado Street: Construction Set

1 message

Denis Maslennikov <dmaslennikov@leabraze.com>

Tue, Sep 24, 2024 at 6:10 PM

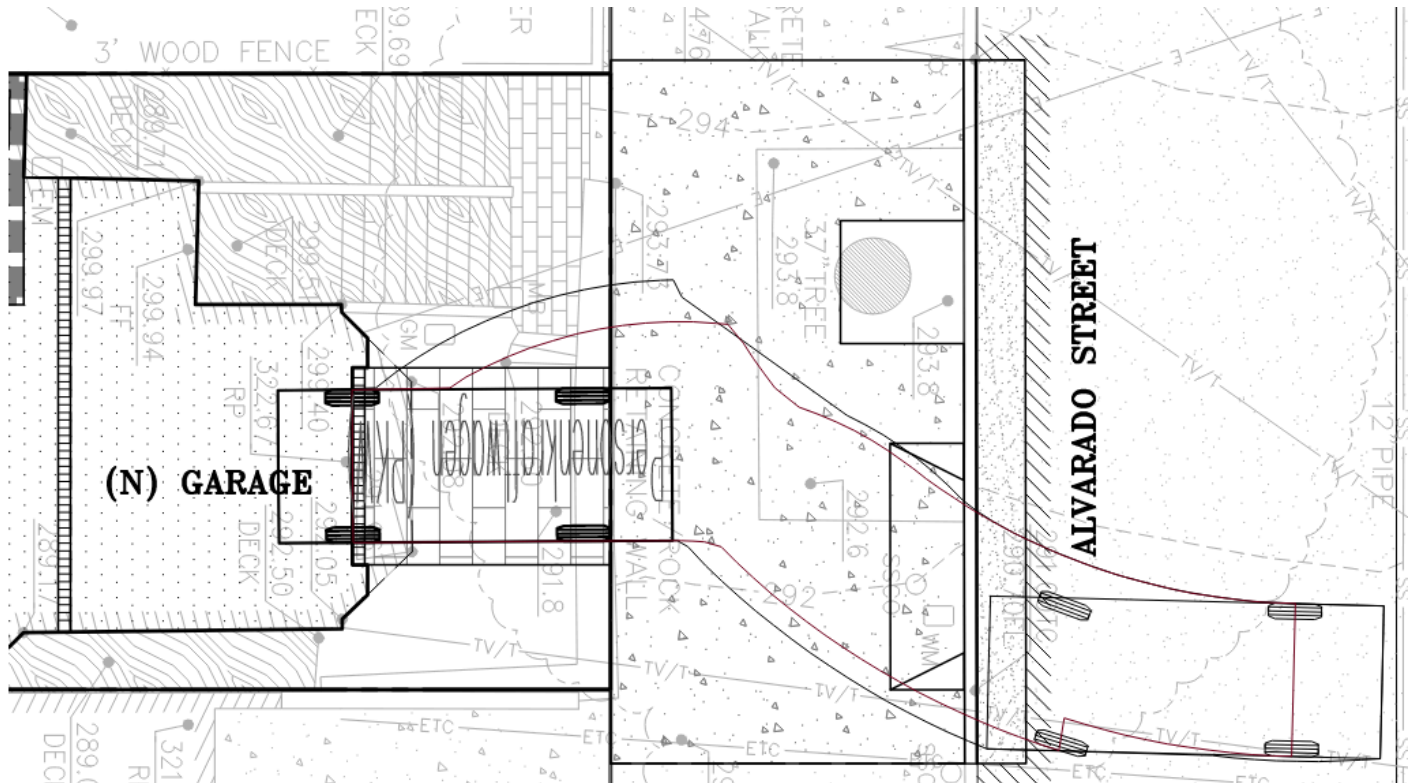
To: Vincent Gonzaga <vincent@vgonzaga.com>

Cc: Chris Del Rosario <cdelrosario@leabraze.com>, Christopher Phan <cphan@leabraze.com>

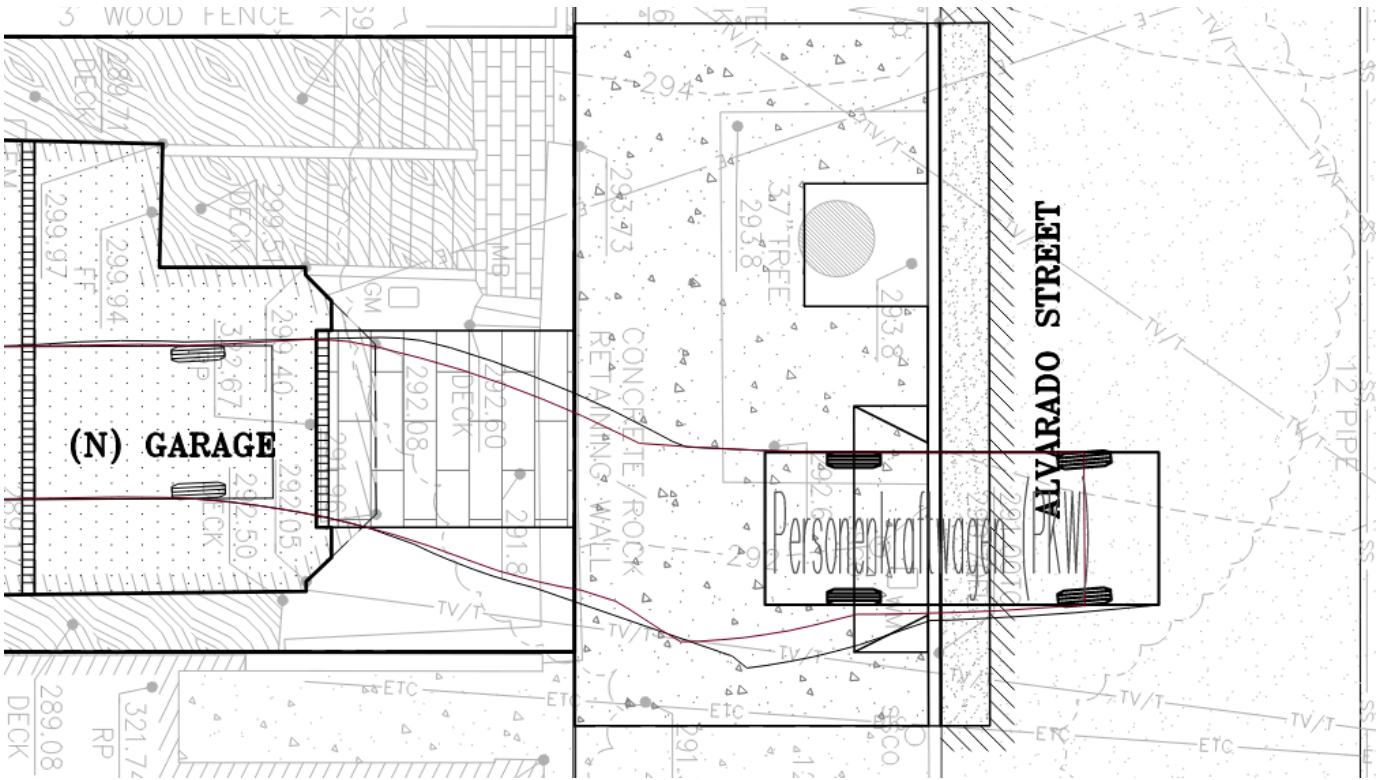
Hi Vincent,

We ran two scenarios using Vehicle Tracking - 1) where the vehicle backs out of the garage and 2) where the vehicle backs into the garage from the street. We picked the smallest car option available in this program, which is 16' long and 6' wide.

When the vehicle reverses out of the garage and clears the garage door, it will need to make a sharp turn that overlaps with the entrance of the house. **Even if you widen the driveway within the property to give the vehicle enough space to turn, it does not make it through the driveway cut.** This is shown in the screenshot below.



When the vehicle reverses into the garage from the street, it barely makes it into the garage, as shown in the screenshot below. But this is also not practical as there is no clearance at the sides of the car, including space for the side view mirrors, and therefore, **the driver has no room for error when making this maneuver.**



In order for a vehicle to comfortably enter/exit the property, the driveway cut needs to align with the garage door as much as possible.

Let me know if you have any questions or need anything else regarding this.

Thanks,

**Denis Maslennikov, P.E.**

Project Manager I



Hayward • Roseville • Pleasanton • San Jose • San Luis Obispo

www.leabraze.com

Phone: 510-887-4086 x144

Direct Line: 510-998-1854

Email: [dmaslennikov@leabraze.com](mailto:dmaslennikov@leabraze.com)

LinkedIn | Instagram | Facebook

**Lea & Braze Engineering Inc., Your Trusted Partner**



# ATTACHMENT 5

1. Window on west side of building does not exist. It was put there illegally by the Murrays in 2004, and covered up and removed when we talked them about it in 2013.
2. Vertical addition on west side of building will have a significant impact on our light. The downstairs kitchen, stairway & bathroom, as well as the upstairs kitchen, living room, stairs & bedroom all have windows along the East side of the building. The vertical addition needs to be set back at least 3ft.
3. The 2 level addition to the back of the house is much higher than anyone else's. It will block our light and view as shown in the following pictures. Everyone else's deck is approximately 20ft above the ground at the back of the house, & follows the slope down the hill from west to east. See attached. I have included a suggestion which results in the same square footage and bedrooms as your plan but which does not impact us.

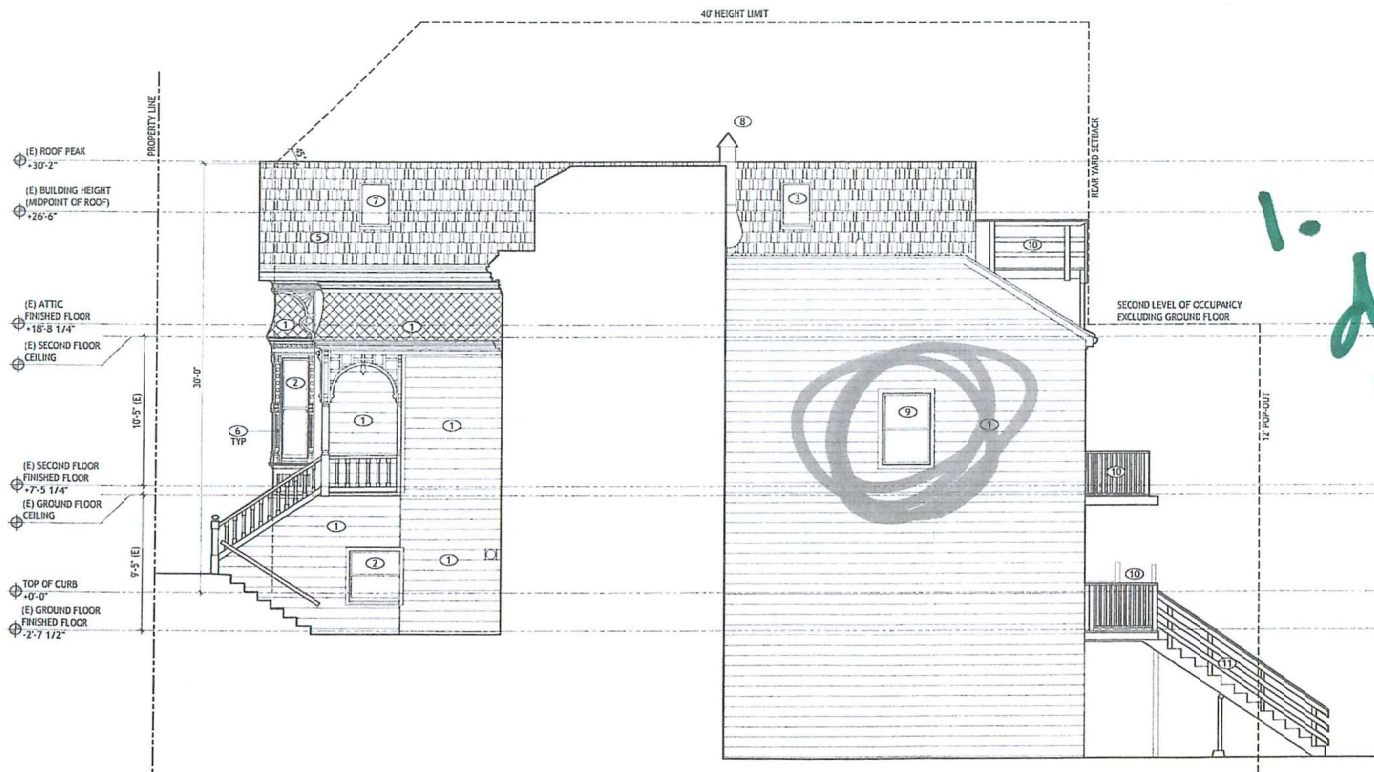


VINCENT  
GONZAGA  
ARCHITECT  
SAN FRANCISCO  
415 596 7738

vincent@gonzaga.com  
1190 PINE ST APT 103  
SAN FRANCISCO CA 94109

KEYED NOTES

- ① (E) SIDING TO BE REPAINTED
- ② (E) WINDOW TO REMAIN
- ③ (E) SKYLIGHT TO BE REMOVED
- ④ (E) WOOD STAIRS, WOOD RAILING & LANDING TO BE REPAINTED
- ⑤ (E) ASPHALT SHINGLE ROOFING TO BE REPLACED TO MATCH (E)
- ⑥ (E) WOOD TRIM TO BE REPAINTED
- ⑦ (E) SKYLIGHT TO BE REPLACED
- ⑧ (E) FIREPLACE FLUE TO BE EXTENDED TO MIN 2'-0" ABOVE ROOF SURFACE WITHIN 10'-0" OF FLUE
- ⑨ (E) WINDOW TO BE REPLACED
- ⑩ (E) WOOD DECK/BALCONY TO BE REMOVED
- ⑪ (E) WOOD STAIRS & LANDING TO BE REMOVED



*1. does not exist*

① EXISTING SIDE (WEST) ELEVATION  
SCALE: 1/4" = 1'-0"

REV#	DESCRIPTION	DATE

PROJECT NAME  
**551 ALVARADO ST  
ADDITION**  
551 ALVARADO ST  
SAN FRANCISCO, CA 94114  
BLOCK/LDT: 1624/039

PROJECT NUMBER 2214  
SHEET TITLE

EXISTING SIDE ELEVATION

SHEET NUMBER  
**A206**



2.

1.







Houses  
to the  
3. West

563

559

decks follow slope down the hill

existing

- KEYED 1
- ① (E) SH
  - ② (E) W
  - ③ (E) S
  - ④ (E) D
  - ⑤ (E) A
  - ⑥ (E) D
  - ⑦ (E) S
  - ⑧ (E) F WITH
  - ⑨ (E) Y
  - ⑩ (E) Y
  - ⑪ (E) Y

deck 563

deck 559



1 EXISTING REAR (SOUTH) ELEVATION  
SCALE: 1/4" = 1'-0"

NEIGHBORING PROPERTY 555-557 ALVARADO ST      SUBJECT PROPERTY 551 ALVARADO ST      NEIGHBORING PROPERTY 547 ALVARADO ST

Should be here  
to line up  
with  
neighbors

563

559



NEIGHBORING PROPERTY  
553-557 ALVARADO ST

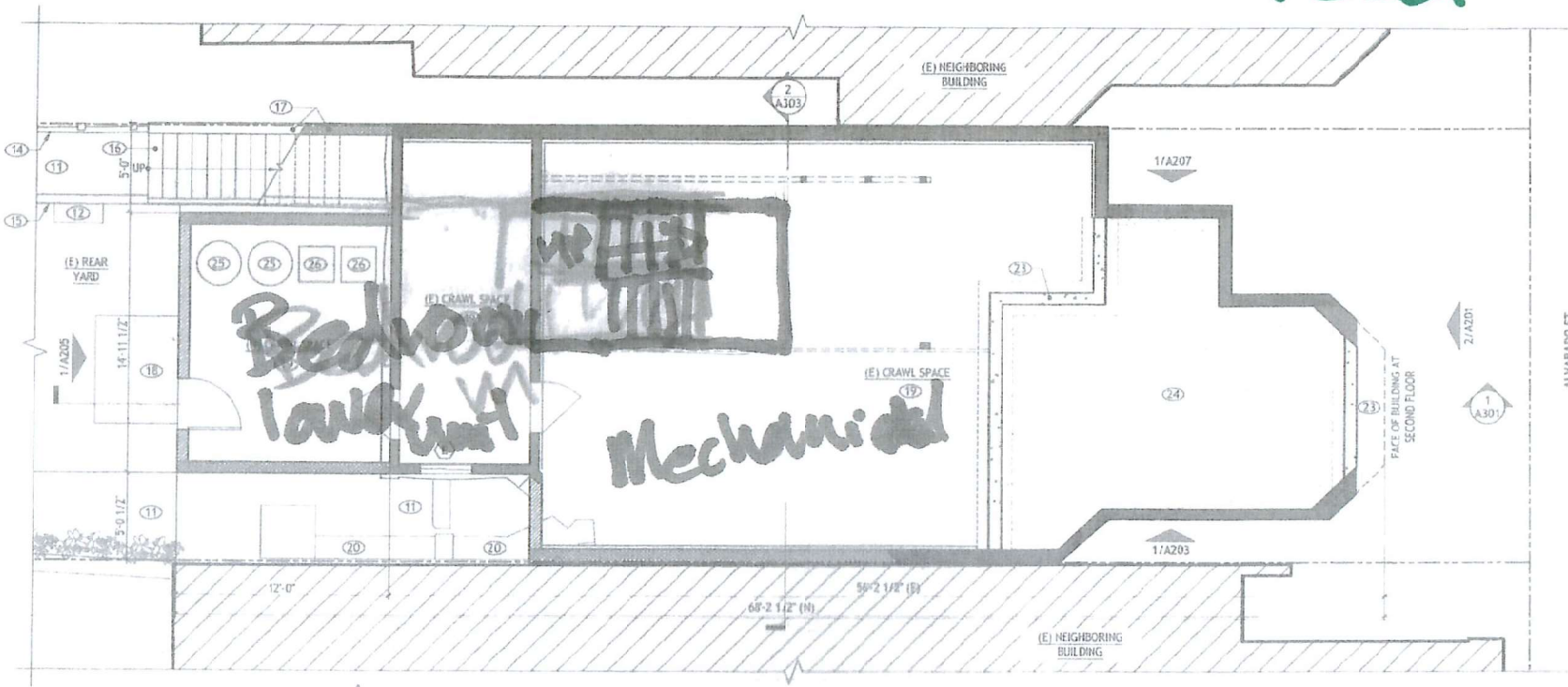
SUBJECT PROPERTY  
551 ALVARADO ST

NEIGHBORING PROPERTY  
547 ALVARADO ST

1 PROPOSED REAR (SOUTH) ELEVATION  
SCALE: 1/4" = 1'-0"



Put Bedroom @ Basement level



2 PROPOSED FOUNDATION LEVEL PLAN  
SCALE: 1/4" = 1'-0"

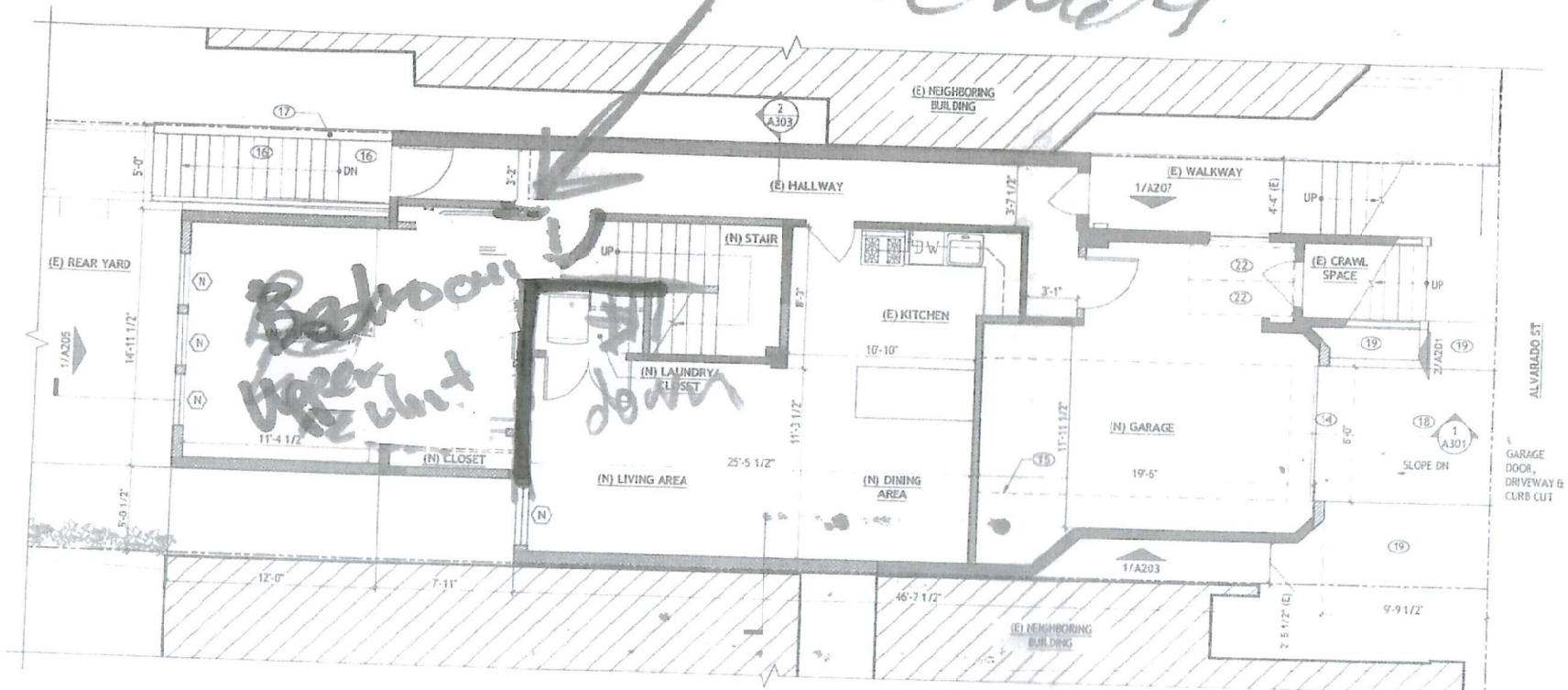
PLAN LEGEND

- 1 REMOVE (E) WALL
- 2 REMOVE (E) WALL
- 3 REMOVE (E) WALL
- 4 REMOVE (E) WALL
- 5 REMOVE (E) WALL
- 6 REMOVE (E) WALL
- 7 REMOVE (E) WALL
- 8 REMOVE (E) WALL
- 9 REMOVE (E) WALL
- 10 AT EXISTING FOUNDATION
- 11 (E) FOUNDATION
- 12 (E) FOUNDATION
- 13 REMOVE (E) WALL
- 14 (E) FOUNDATION
- 15 (E) FOUNDATION
- 16 (E) FOUNDATION
- 17 (E) FOUNDATION
- 18 (E) FOUNDATION
- 19 (E) FOUNDATION
- 20 (E) FOUNDATION
- 21 (E) FOUNDATION
- 22 (E) FOUNDATION
- 23 (E) FOUNDATION
- 24 (E) FOUNDATION
- 25 (E) FOUNDATION
- 26 (E) FOUNDATION

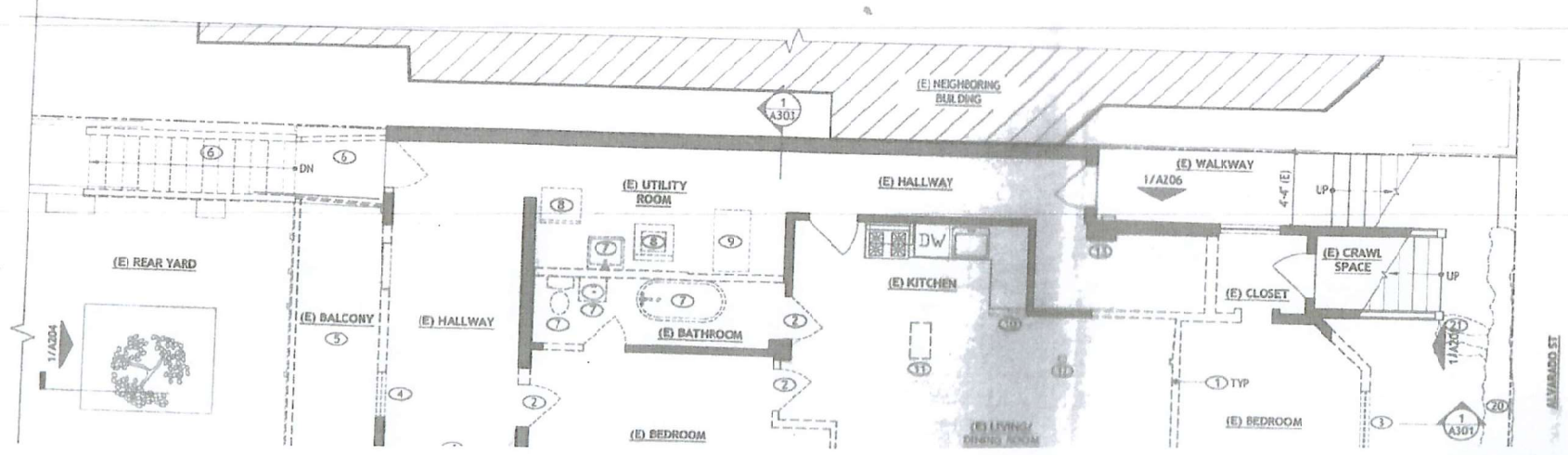
KEYED NOTES

- 1 REMOVE (E) WALL
- 2 REMOVE (E) WALL
- 3 REMOVE (E) WALL
- 4 REMOVE (E) WALL
- 5 REMOVE (E) WALL
- 6 REMOVE (E) WALL
- 7 REMOVE (E) WALL
- 8 REMOVE (E) WALL
- 9 REMOVE (E) WALL
- 10 AT EXISTING FOUNDATION
- 11 (E) FOUNDATION
- 12 (E) FOUNDATION
- 13 REMOVE (E) WALL
- 14 (E) FOUNDATION
- 15 (E) FOUNDATION
- 16 (E) FOUNDATION
- 17 (E) FOUNDATION
- 18 (E) FOUNDATION
- 19 (E) FOUNDATION
- 20 (E) FOUNDATION
- 21 (E) FOUNDATION
- 22 (E) FOUNDATION
- 23 (E) FOUNDATION
- 24 (E) FOUNDATION
- 25 (E) FOUNDATION
- 26 (E) FOUNDATION

*remove hallway*



2 PROPOSED GROUND FLOOR PLAN  
SCALE: 1/4" = 1'-0"



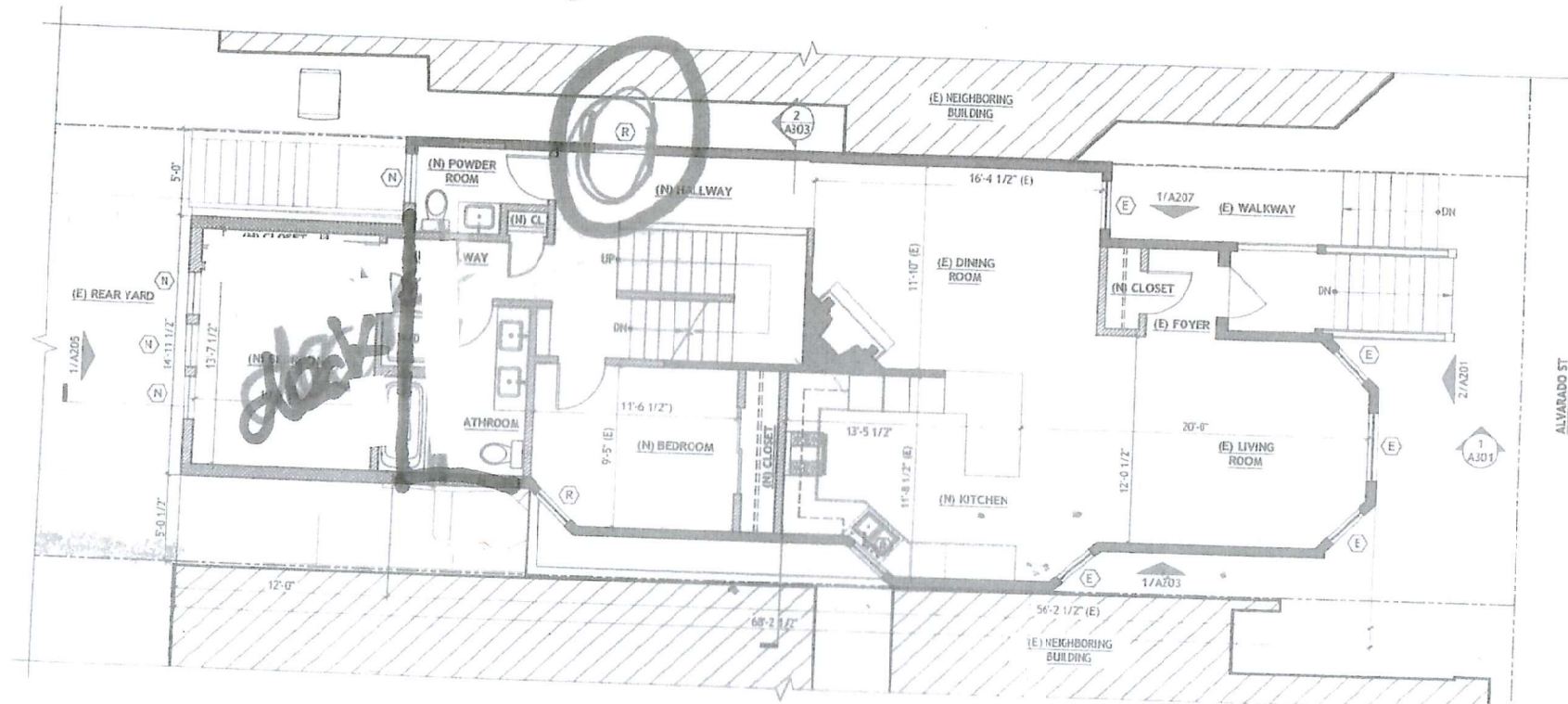
PLAN LEGEND



KEYED NOTES

- 1 REMOVE (E) REAR YARD
- 2 REMOVE (E) BALCONY
- 3 REMOVE (E) UTILITY ROOM
- 4 REMOVE (E) BATHROOM
- 5 REMOVE (E) KITCHEN
- 6 REMOVE (E) DINING AREA
- 7 REMOVE (E) LIVING AREA
- 8 REMOVE (E) LAUNDRY
- 9 REMOVE (E) STAIR
- 10 REMOVE (E) GARAGE
- 11 REMOVE (E) CRAWL SPACE
- 12 REMOVE (E) HALLWAY
- 13 REMOVE (E) BEDROOM
- 14 REMOVE (E) BEDROOM
- 15 REMOVE (E) CLOSET
- 16 REMOVE (E) WALKWAY
- 17 REMOVE (E) NEIGHBORING BUILDING
- 18 REMOVE (E) DRIVEWAY
- 19 REMOVE (E) CURB CUT
- 20 REMOVE (E) ALVARADO ST

deck at this level



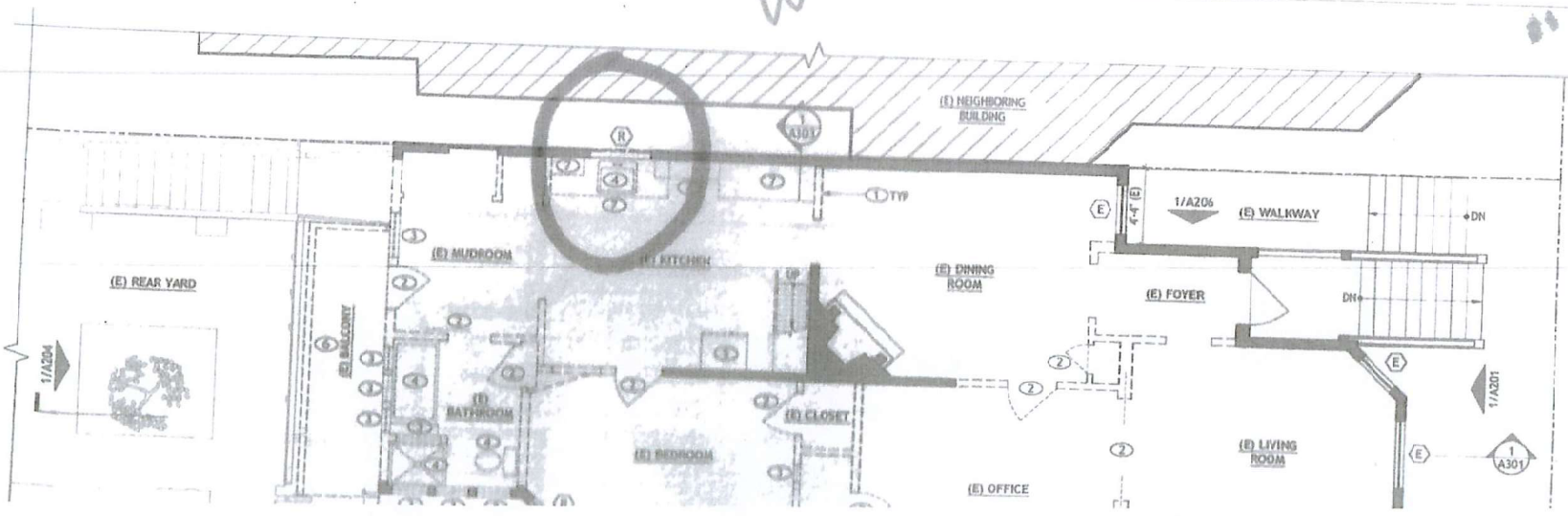
PLAN LEGEND

- EXISTING CEILING
- EXISTING FLOOR
- NEW FLOOR
- NEW FIRE PARTITION
- EXISTING DOOR
- NEW DOOR
- NEW WINDOW
- NEW PARTITION
- NEW CABINETS

KEYED NOTES

- 1 REMOVE (E) PARTITION
- 2 REMOVE (E) DOOR, DOOR FRAME
- 3 REMOVE (E) WINDOW
- 4 REMOVE (E) PLUMBING SYSTEM
- 5 REMOVE (E) APPLIANCE
- 6 REMOVE (E) BALCONY
- 7 REMOVE (E) CABINETS

2 PROPOSED SECOND FLOOR PLAN  
SCALE: 1/4" = 1'-0"



ALVARADO ST

2



VINCENT  
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ARCHITECT  
SAN FRANCISCO  
415 650 7228  
vinceit@gonzaga.com  
1190 PINE ST APT 105  
SAN FRANCISCO CA 94109

KEYED NOTES

- 1 (E) PAINTED WOOD SIDING TO MATCH (E)
- 2 (N) WINDOW
- 3 (E) SKYLIGHT TO BE REMOVED
- 4 (N) DOOR
- 5 (N) PAINTED WOOD TRIM TO MATCH (E)
- 6 (N) WOOD STAIRS & RAILING
- 7 (N) FIRE RATED GUARDRAIL
- 8 (E) FIREPLACE FLUE TO BE EXTENDED TO MIN 2'-0" ABOVE ROOF SURFACE WITHIN 10'-0" OF FLUE
- 9 (N) ASPHALT SHINGLE ROOFING TO MATCH (E)
- 10 (E) WINDOW TO BE REPLACED
- 11 (N) PAINTED WOOD GUARDRAIL
- 12 (N) GUTTER
- 13 (N) DOWNSPOUT

*Vertical addition  
3' + ~~2'~~ move  
away from  
1 is ht well deck at  
this level*



1 PROPOSED REAR (SOUTH) ELEVATION  
SCALE: 1/4" = 1'-0"

*Bedroom of lower unit*

PRE-APPLICATION MEETING 02/14/23  
REV# DESCRIPTION DATE  
ISSUE DATES / REVISIONS

PROJECT NAME

551 ALVARADO ST  
ADDITION  
551 ALVARADO ST  
SAN FRANCISCO, CA 94114  
BLOCK/LOT: 3624/039

PROJECT NUMBER 2214  
SHEET TITLE

PROPOSED REAR ELEVATION

SHEET NUMBER

A205

# ATTACHMENT 6

## 551 Alvarado Street

### Design Changes per Comments Received from C. Woods at the Pre-Application Meeting

1. The basement was originally designed to be used as crawlspace and storage areas. The original design kept construction costs low and simplified fire rating between dwelling units.
2. The lower dwelling unit was located completely on the ground floor.
3. To accommodate C. Woods' comments regarding the height of the rear pop out, the lower dwelling unit was split so that the bedroom was relocated to the basement and one of the upper dwelling unit bedrooms was relocated to the ground floor from the second floor.
4. To accommodate C. Wood's comments requesting that the third floor vertical addition be located 3' from her property line, the stair, third floor and ground floor were all reconfigured.
5. Splitting the dwelling units so that portions of the upper and lower dwelling unit were located on the ground floor required complicated detailing to maintain the fire-rating between the dwelling units.
6. Splitting the dwelling units also required more complicated mechanical duct routing to avoid and minimize ductwork passing through fire-rated construction.
7. The complicated dwelling unit separation required extra drawings and discussions with the plan checker to explain how fire-rated separation between the dwelling units would be achieved.
8. Setting back the third floor vertical addition from the property line contributed to a more complicated structural design.
9. The Owner of 551 Alvarado voluntarily made these changes at substantial expense for design fees and increase in construction cost.



# ATTACHMENT 7

23IE-00435

## Street Improvement Permit

Address : 551 ALVARADO ST

Cost: \$1,827.50

Block:3624 Lot: 039 Zip: 94114

Pursuant to article 2.4 of the Public Works Code in conjunction to DPW Order 187,005, permission, revocable at the will of the Director of Public Works, to construct improvements within the public right-of-way is granted to Permittee. Major events in San Francisco the week of November 12, 2023, may necessitate street closures and traffic rerouting. As a result, Public Works-issued permits allowing work in the public right of way may be suspended temporarily per Public Works Code section 2.4.54 and section 724(f), starting as early as November 1, 2023. Once we have more details on the potential impacts, Public Works will contact you if your permit will be affected. Thank you for your understanding.

### JS Concrete Construction

Name: JS Concrete Construction

#### Conditions

Permittee shall coordinate all work with other construction projects and events known or unforeseen such as to minimize the impact of construction project on the general public and/or event(s).

Mandatory Coordination with conflicting permits is required. Permit holder shall not commence work without first properly coordinating with existing permit holders as noted on the exception page(s) of this permit. If this permit conflicts with a city project or other approved permits, the permit holder of this permit shall be responsible for proper coordination and evaluation of the site prior to commencing work.

The permittee shall comply with all existing traffic controls and parking restrictions. The permittee shall also comply with any additional restrictions under the Special Traffic Permit issued by SFMTA. For information related to construction traffic restrictions please reference the latest edition of Regulations for Working in San Francisco Streets, the Blue Book. To download a copy of the Blue Book, please visit <https://www.sfmta.com/services/streets-sidewalks/construction-regulations>.

A separate Street Space Permit is required for construction staging.

NTR 0

Curb Cut Sq Footage 30

Completion Work shall be completed, inspected and signed off by inspector by the expiration date

<b>Remove, replace or reconstruct:</b>	Remove and reconstruct new (10') driveway curb cut and sidewalk per approved plan, saw-cut 2' of AC gutter and replace with 2" ACWS over 8" concrete base. Additional paving as required and directed by SFPW/BSM inspector. Field inspection is MANDATORY prior to excavation and pouring concrete, call 1-628-271-2000 or email dpw-bsminspects@sfdpw.org to schedule.
<b>Expiration Date</b>	9/15/2025
<b>Bond Amount:</b>	0
<b>Linear Footage</b>	0
<b>Bond Holder:</b>	
<b>Contact</b>	247 Refer to Agent
<b>DPW Resolution #</b>	
<b>Inspection</b>	Work shall not commence until this permit has been activated by Public Works. The permittee shall contact Public Works at dpw-bsminspects@sfdpw.org or (628) 271-2000 to activate the permit and schedule inspection at least 72 hours prior to work. Failure to follow the activation process prior to commencing work may result in a correction notice and possible notice of violation.

The undersigned Permittee hereby agrees to comply with all requirements and conditions noted on this permit

**Approved Date : 07/17/2024**

Excavation and grading of subject area for street reconstruction shall be in accordance with approved plans and City specifications. Damaged areas adjacent to this construction shall be properly patched per City Inspector. Also, the permittee shall be responsible for any ponding due to the permitted work.

\_\_\_\_\_  
Applicant/Permittee

\_\_\_\_\_  
Date

Distribution:  
Outside BSM: BOE (Streets and Hyws) - P. Riviera  
Inside BSM: Street Improvement Inspection

Printed : 7/18/2024 9:10:10 AM

Plan Checker

Berhane Gaime

## STREET EXCAVATION REQUIREMENTS

1. The permittee shall call Underground Service Alert (U.S.A.), telephone number 811, 48 hours prior to any excavation.
2. All work including sidewalk and pavement cutting and removal, lagging, excavation, backfill, and sidewalk and pavement restoration shall be done by a licensed paving contractor and in accordance with the requirements of the Current Standard Specifications of Public Works.
3. All work including sidewalk and pavement cutting and removal, lagging, excavation, backfill, and sidewalk and pavement restoration shall be done by a licensed contractor and in accordance with the requirements of the latest edition of Standard Specifications and Plans of San Francisco Public Works, and Department of Public Works Order Nos. 187,005.
4. Sidewalk and pavement restoration shall include the replacement of traffic lane and crosswalk striping, parking stall markings, and curb painting that might have been obliterated during street excavation. The permittee shall perform their work under on the following options:
  - a. Have the City forces do the striping and painting work at the permittees expense. The permittee shall make a deposit with the Department of Parking & Traffic for this purpose in an amount estimated by the Municipal Transportation Agency (MTA) 7th Floor 1 South Van Ness Ave telephone 701-4500, and notify the MTA at least 48 hours in advance of the time the work is to be done.
  - b. Perform the work themselves following instructions available at the Department of Parking & Traffic and MTA.
5. The permittee shall submit a non-refundable fee to Bureau of Street-Use and Mapping to pay for City Inspection of the backfill and pavement restoration. At least 48 hours in advance, the permittee shall make arrangements with the Street Improvement Section Inspectors, (628) 271-2000, for an inspection schedule.
6. The permittee shall file and maintain an excavation bond in the sum of \$25,000.00 with the Department of Public Works, to guarantee the maintenance of the pavement in the excavation area for a period of 3 years following the completion of the backfill and pavement restoration pursuant to Article 2.4.40 of the Public Works Code.
7. The permittee shall conduct construction operations in accordance with the requirements of Article 900 Section 903(a) and (b) of the Traffic Code. The permittee shall contact the MTA 7th Floor 1 South Van Ness Ave telephone 701-4500, for specific restrictions before starting work.
8. The permittee shall obtain the required permits, if any, from regulating agencies of the State of California.
9. The permittee shall verify the locations of any City or public service utility company facilities that may be affected by the work authorized by this permit and shall assume all responsibility for any damage to such facilities. The permittee shall make satisfactory arrangements and payments for any necessary temporary relocation of City or public utility company facilities.
10. The permittee shall pay the required fee for sewer installation permit at the Plumbing Inspection Division, Department of Building Inspection, 1660 Mission Street and arrange for inspection of this work, telephone 558-6054.
11. Planting of trees and performance of any work in the right-of-way which may affect a tree and/or landscaping shall not be performed prior to obtaining a permit and/or another form of approval from Bureau of Urban Forestry (BUF), telephone: (628) 652-8733.
12. Per DPW Order 201,954, the recycling of Cobble Stones and Granit Curb shall follow as:
  - a. Cobblestones shall be clean of dirt prior to transporting. Extreme care shall be taken during the transporting the cobblestones to minimize damage before delivery to City. The cobblestones shall be neatly and securely placed on pallets so they can be moved about safely after the delivery. The Minimum size of cobblestone shall be 4 inches square (16 square inches). The cobblestones shall be delivered, including off loading, to 701 14th Street on Treasure Island or at alternative location directed by the Department within the City of San Francisco. Contact the Department forty-eight hours (48 hours) prior to delivery. The Department can be reached at (415) 641-2627.
  - b. Granite Curb shall be neatly and securely placed on pallets so they can be moved about safely after delivery. The Contractor shall exercise care in transporting the granite curb to minimize damage. The length limit of recyclable granite curbs shall be no less than four feet. The granite curb shall be delivered, including off loading, to 701 14th Street on Treasure Island or at an alternative location directed by the Department within the City of San Francisco. Contact Bureau of Street and Sewer Repair (BSSR) at least forty-eight hours (48 hours) prior to delivery. BSSR can be reached at (415) 695-2087.
13. In consideration of this Permit being issued for the work described in the application, Permittee on its behalf and that of any successor or assign, and on behalf of any lessee, promises and agrees to perform all the terms of this Permit and to comply with all applicable laws, ordinances and regulations.
14. Permittee agrees on its behalf and that of any successor or assign to hold harmless, defend, and indemnify the City and County of San Francisco, including, without limitation, each of its commissions, departments, officers, agents and employees (hereinafter collectively referred to as the "City") from and against any and all losses, liabilities, expenses, claims, demands, injuries, damages, fines, penalties, costs or judgments including, without limitation, attorneys' fees and costs (collectively, "claims") of any kind allegedly arising directly or indirectly from (i) any act by, omission by, or negligence of, Permittee or its subcontractors, or the officers, agents, or employees of either, while engaged in the performance of the work authorized by this Permit, or while in or about the property subject to this Permit for any reason connected in any way whatsoever with the performance of the work authorized by this Permit, or allegedly resulting directly or indirectly from the maintenance or installation of any equipment, facilities or structures authorized under this Permit, (ii) any accident or injury to any contractor or subcontractor, or any officer, agent, or employee of either of them, while engaged in the performance of the work authorized by this Permit, or while in or about the property, for any reason connected with the performance of the work authorized by this Permit, or arising from liens or claims for services rendered or labor or materials furnished in or for the performance of the work authorized by this Permit, (iii) injuries or damages to real or personal property, good will, and persons in, upon or in any way allegedly connected with the work authorized by this Permit from any cause or claims arising at any time, and (iv) any release or discharge, or threatened release or discharge, of any hazardous material caused or allowed by Permittee in, under, on or about the property subject to this Permit or into the environment. As used herein, "hazardous material" means any substance, waste or material which, because of its quantity, concentration of physical or chemical characteristics is deemed by any federal, state, or local governmental authority to pose a present or potential hazard to human health or safety or to the environment.
15. Permittee must hold harmless, indemnify and defend the City regardless of the alleged negligence of the City or any other party, except only for claims resulting directly from the sole negligence or willful misconduct of the City. Permittee specifically acknowledges and agrees that it has an immediate and independent obligation to defend the City from any claim which actually or potentially falls within this indemnity provision, even if the allegations are or may be groundless, false or fraudulent, which obligation arises at the time such claim is tendered to Permittee by the City and continues at all times thereafter. Permittee agrees that the indemnification obligations assumed under this Permit shall survive expiration of the Permit or completion of work.
16. Permittee shall obtain and maintain through the terms of this Permit general liability, automobile liability or workers' compensation insurance as the City deems necessary to protect the City against claims for damages for personal injury, accidental death and property damage allegedly arising from any work done under this Permit. Such insurance shall in no way limit Permittee's indemnity hereunder. Certificates of insurance, in form and with insurers satisfactory to the City, evidencing all coverages above shall be furnished to the City before commencing any operations under this Permit, with complete copies of policies furnished promptly upon City request.
17. The permittee and any permitted successor or assign recognize and understand that this permit may create a possessory interest.
18. Separate permit is required for excavation of side sewers. Installation authorized only by Class "A" or "C-42" Licensed Contractor or "C-12" with "C-36" Licensed Contractor. Authorization requires the filing of a \$25,000 excavation bond to cover the cost of City inspection and having obtained authorization to excavate in the roadway. The contractor shall obtain the proper permits and arrange for an inspection, for the section of pipe from the trap to the property, with the Plumbing Inspection Division at 1660 Mission Street, telephone 558-6054.
19. Pursuant to state law, all survey monuments must be preserved. No work (including saw cutting) may commence within 20' of a survey monument until an application for Monument Referencing has been approved and notification of monument referencing has occurred. Prior to construction, all CCSF survey monuments shall be referenced by a licensed Land Surveyor on a Corner Record or Record of Survey if any construction will take place within 20 ft. of a monument. For any questions please email Monument.Preservation@sfdpw.org or call 415-554-5827. Note, all survey monuments shall be preserved per state law and disturbance of a survey monument is a crime.



Not all survey monuments are visible.

20. If sidewalk finish grade is to be altered, permittee shall replace, relocate and reset pull boxes as necessary to ensure pull boxes are flush with sidewalk, not damaged and not covered, filled or sprayed with concrete and debris, etc. Permittee shall also raise or lower street light, signal poles and other city poles/infrastructure as necessary to ensure anchor bolts and hand holes are not buried or that base of pole is elevated above sidewalk surface. For additional guidance street light and signal poles matters, consult Transportation Engineer Norman Wong at [norman.wong@sfmta.com](mailto:norman.wong@sfmta.com).

# Permit Addresses

23IE-00435

\*RW = RockWheel, SMC = Surface Mounted Cabinets, S/W = Sidewalk Work, DB = Directional Boring,  
 BP= Reinforced Concrete Bus Pad, UB = Reinforced Concrete for Utility Pull Boxes and Curb Ramps  
 Green background: Staging Only

Number of blocks: 1    Total repair size:0 sqft    Total Streetspace:0    Total Sidewalk: sqft

ID	Street Name	From St	To St	Sides	*Other	Asphalt	Concrete	Street Space Feet	Sidewalk Feet
1	ALVARADO ST	NOE ST	CASTRO ST	Odd	RW : False SMC : False S/W Only : False DB: False BP: False UB: False	0	0	0	
<b>Total</b>						<b>0</b>	<b>0</b>	<b>0</b>	

# Exceptions

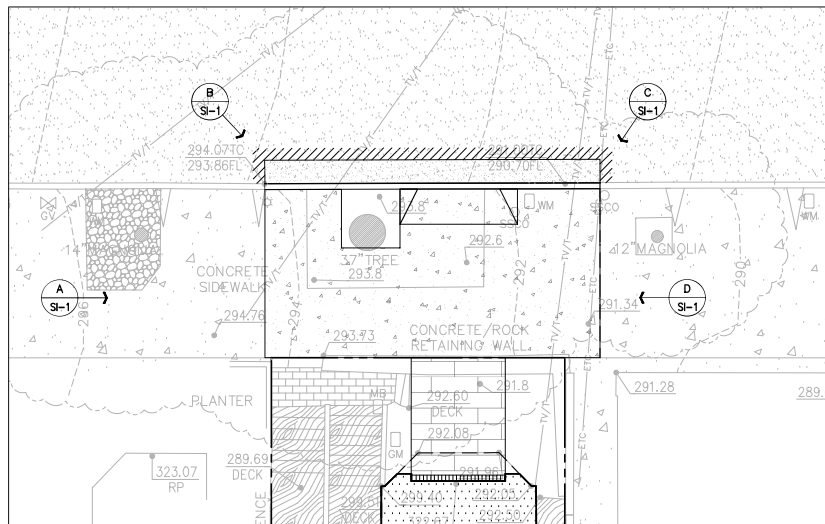
23IE-00435

Street Name	From St	To St	Message	Job	Contact	Dates
ALVARADO ST						
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	12MSE-0298	Refer to Agent - Refer to Agent	
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	14CN-0143	415 309 0107 - 415 309 0107	
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	14ECN-0088	800-743-5000 - 800-743-5000	Jan 14 2014-
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	20CN-00111	(843) 709-2136 - (843) 709- 2136	
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	20CN-00133	Refer to Agent - Refer to Agent	
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	20MSE-00492	Refer to Agent - Refer to Agent	
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	21ECN-00088	Refer to Agent 415 550 4956 - 415-550-4900	Feb 2 2021-
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	21ECN-00094	Refer to Agent 415 550 4956 - 415-550-4900	Feb 3 2021-
	NOE ST	CASTRO ST -	Conflict with existing Street Use Permit.	23ECN-00124	Oscar Delgadillo @ 510-867- 0073 - (415) 337-2934	Feb 16 2023-
	NOE ST	CASTRO ST -	Proposed Excavation.	SF DPW IDC - Hydraulic	Satnam Kaur -	Nov 15 2024-Aug 25 2025

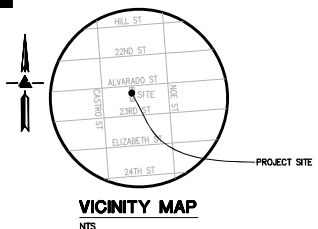
# No Diagram submitted

# OFF-SITE IMPROVEMENTS LAROCHIE & LAUDON RESIDENCE 551 ALVARADO STREET SAN FRANCISCO, CALIFORNIA

EXISTING	PROPOSED	DESCRIPTION
---	---	BOUNDARY
---	---	PROPERTY LINE
---	---	RETAINING WALL
---	---	LANDSCAPE RETAINING WALL
---	---	RAINWATER TIGHTLINE
---	---	SUBDRAIN LINE
---	---	TIGHTLINE
---	---	STORM DRAIN LINE
---	---	SANITARY SEWER LINE
---	---	WATER LINE
---	---	GAS LINE
---	---	STORM DRAIN PRESSURE LINE
---	---	SANITARY SEWER PRESSURE LINE
---	---	JOINT TRENCH
---	---	SET BACK LINE
---	---	CONCRETE VALLEY GUTTER
---	---	EARTHEN SWALE
---	---	CATCH BASIN
---	---	JUNCTION BOX
---	---	AREA DRAIN
---	---	CURB INLET
---	---	STORM DRAIN MANHOLE
---	---	FIRE HYDRANT
---	---	SANITARY SEWER MANHOLE
---	---	STREET SIGN
---	---	SPOT ELEVATION
---	---	FLOW DIRECTION
---	---	DEMOLISH/REMOVE
---	---	BENCHMARK
---	---	CONTOURS
---	---	TREE TO BE REMOVED
---	---	TREE PROTECTION FENCING



**KEY MAP**  
1" = 5'



**VICINITY MAP**  
NTS

**OWNER'S INFORMATION**  
OWNER: RANDALL LAROCHE & DAVID LAUDON  
551 ALVARADO STREET  
SAN FRANCISCO, CA

APN: 3624-039

**REFERENCES**

- THIS GRADING AND DRAINAGE PLAN IS SUPPLEMENTAL TO:
1. TOPOGRAPHIC SURVEY BY LEA & BRAZE ENGINEERING INC. ENTITLED: "TOPOGRAPHIC SURVEY" 551 ALVARADO STREET SAN FRANCISCO, CA DATE: 10-27-22 JOB# 2221580
  2. SITE PLAN BY VINCENT GONZAGA ARCHITECT ENTITLED: "551 ALVARADO ST ADDITION" 551 ALVARADO STREET SAN FRANCISCO, CA
- THE CONTRACTOR SHALL REFER TO THE ABOVE NOTED SURVEY AND PLAN, AND SHALL VERIFY BOTH EXISTING AND PROPOSED ITEMS ACCORDING TO THEM.

**NOTE:**  
FOR CONSTRUCTION STAKING SCHEDULING OR QUOTATIONS PLEASE CONTACT ALEX ABAYA AT LEA & BRAZE ENGINEERING (510)887-4086 EXT 116, aabaya@leabraze.com

**\* BUILDING PAD NOTE:**  
ADJUST PAD LEVEL AS REQUIRED. REFER TO STRUCTURAL PLANS FOR SLAB SECTION OR CRAM. SPACE DEPTH TO ESTABLISH PAD LEVEL.



**LEA & BRAZE ENGINEERING, INC.**  
CIVIL ENGINEERS | LAND SURVEYORS  
REGISTERED PROFESSIONAL ENGINEERS  
REGISTERED PROFESSIONAL SURVEYORS  
HAYWARD, CALIFORNIA 94545  
(925) 887-4086  
WWW.LEABRAZE.COM

LAROCHE & LAUDON  
RESIDENCE  
551 ALVARADO STREET  
SAN FRANCISCO, CALIFORNIA  
APN: 3624-039

**ABBREVIATIONS**

AB	AGGREGATE BASE	LF	LINEAR FEET
ACC	ASPHALT CONCRETE	MAX	MAXIMUM
AD	ACCESSIBLE	MH	MANHOLE
AD	AREA DRAIN	MIN	MINIMUM
B & D	BEGINNING OF CURVE	MON.	MONUMENT
BC	BEARING & DISTANCE	MO	METERED RELEASE OUTLET
BM	BENCHMARK	(N)	NEW
BUB	BUBBLER BOX	NO.	NUMBER
BW/FG	BOTTOM OF WALL/FINISH GRADE	NTS	NOT TO SCALE
CB	CATCH BASIN	O.C.	ON CENTER
C & G	CURB AND GUTTER	O/J	OVER
E	CENTER LINE	(PA)	PLANTING AREA
OPP	CORRUGATED PLASTIC PIPE (SMOOTH INTERIOR)	PE	PEDESTRIAN
CO	CLEANOUT TO GRADE	PV	POST INDICATOR VALVE
COTG	CLEANOUT TO GRADE	PSS	PUBLIC SERVICES EASEMENT
CONC	CONCRETE	PP	PROPERTY LINE
CONST	CONSTRUCT OR -TION	PP	POWER POLE
CONC COR	CONCRETE CORNER	PUE	PUBLIC UTILITY EASEMENT
CY	CUBIC YARD	PVC	POLYVINYL CHLORIDE
D	DIAMETER	R	RADIUS
DI	DROP INLET	ROP	REINFORCED CONCRETE PIPE
DIP	DUOTILE IRON PIPE	RM	RISE ELEVATION
EA	EACH	RW	RAINWATER
EA	END OF CURVE	R/W	RIGHT OF WAY
EG	EXISTING GRADE	S	SLOPE
EL	ELEVATIONS	S.A.D.	SEE ARCHITECTURAL DRAWINGS
EQ	EDGE OF PAVEMENT	SAN	SANITARY
EQ	EQUIPMENT	SD	STORM DRAIN
EW	EACH WAY	SDM	STORM DRAIN MANHOLE
(E)	EXISTING	SHT	SHEET
FF	FACE OF CURB	S.I.D.	SEE LANDSCAPE DRAWINGS
FF	FINISHED FLOOR	SPEC	SPECIFICATION
FG	FINISHED GRADE	SS	SANITARY SEWER
FH	FIRE HYDRANT	SSCO	SANITARY SEWER CLEANOUT
FL	FLOW LINE	SSMH	SANITARY SEWER MANHOLE
FS	FINISHED SURFACE	ST	STREET
GA	GAGE OR GAUGE	STA	STATION
GB	GRADE BREAK	STR	STRUCTURAL
GDPE	HIGH DENSITY CORRUGATED POLYETHYLENE PIPE	TEL	TELEPHONE
HORIZ	HORIZONTAL	TC	TOP OF CURB
HI FT	HIGH POINT	TCW	TOP OF WALL
H&T	HUB & TACK	TEMP	TEMPORARY
ID	INSIDE DIAMETER	TP	TOP OF PAVEMENT
INV	INVERT ELEVATION	TP/FG	TOP OF WALL/FINISH GRADE
JB	JUNCTION BOX	TP	TYPICAL
JT	JOINT TRENCH	VC	VERTICAL CURVE
JP	JOINT UTILITY POLE	VCP	VITRIFIED CLAY PIPE
L	LENGTH	VERT	VERTICAL
LANDG	LANDING	W	WITH
		W.W.	WATER LINE
		WM	WATER METER
		WFF	WELDED WIRE FABRIC

**BENCHMARK**

CITY OF SAN FRANCISCO BENCHMARK 11695 SOUTHEAST CORNER HOE STREET AND 23RD STREET. CCSF STANDARD 1/2" DOMED STAINLESS STEEL ANCHOR SCREW WITH WASHER STAMPED "CCSF CONTROL" ELEVATION = 224.952' (NAVD 88 DATUM)

**UTILITY NOTE**

ALL UNDERGROUND PIPE TYPES, SIZES AND LOCATION SHOWN ON THIS SURVEY ARE BASED ON VISUAL OBSERVATION. ANY USE OF THIS INFORMATION SHOULD BE VERIFIED, BEFORE ITS USE, WITH THE CONTROLLING MUNICIPALITY OR UTILITY PROVIDER. THIS SURVEY MAKES NO GUARANTEE OF THE INSTALLED ACTUAL LOCATION, DEPTHS OR SIZE.

**EASEMENT NOTE**

THERE ARE NO EASEMENTS LISTED IN TITLE REPORT ISSUED BY NORTH AMERICAN TITLE COMPANY, INC., ORDER NO. 56066-21-04020, DATED APRIL 26, 2022.

**SITE BENCHMARK**

SURVEY CONTROL POINT MAG AND SHINER SET IN ASPHALT ELEVATION = 288.00' (NAVD 88 DATUM)

**TREE NOTE**

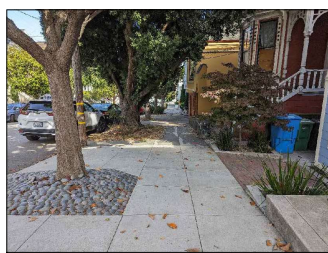
TREE SIZE, TYPE AND DRIPLINES ARE BASED ON A VISUAL OBSERVATION. FINAL DETERMINATION SHOULD BE MADE BY THE PROJECT ARCHITECT.

**NOTES**

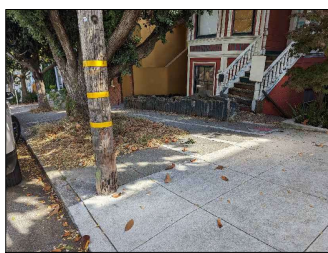
- ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS.
- BUILDING FOOTPRINTS ARE SHOWN TO FINISHED MATERIAL (STUCCO/SIDING) AT GROUND LEVEL.
- FINISH FLOOR ELEVATIONS ARE TAKEN AT DOOR THRESHOLD (EXTERIOR).
- THE AREA OF THE SURVEYED LOT IS 2,850± SQUARE FEET / 0.065± ACRES

**SHEET INDEX**

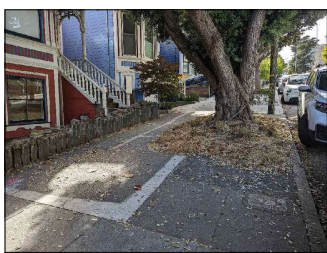
SI-1.0	STREET IMPROVEMENTS TITLE SHEET
SI-2.0	STREET IMPROVEMENTS GRADING & DRAINAGE PLAN
SI-3.0	STREET IMPROVEMENTS UTILITY PLAN
SI-4.0	STREET IMPROVEMENTS DETAILS
SI-4.1	STREET IMPROVEMENTS DETAILS
SI-5.0	STREET IMPROVEMENTS GRADING SPECIFICATIONS
SI-5.1	STREET IMPROVEMENTS GRADING SPECIFICATIONS
ER-1	STREET IMPROVEMENTS EROSION CONTROL PLAN



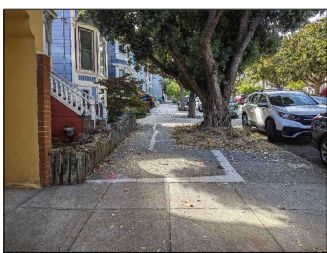
**A FACING EAST**



**B FACING SOUTHEAST**



**C FACING SOUTHWEST**



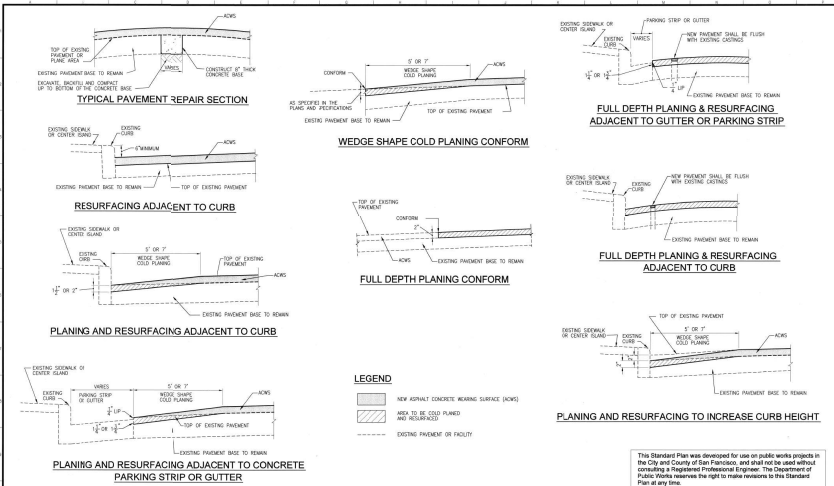
**D FACING WEST**

STREET IMPROVEMENTS  
TITLE SHEET

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REVISIONS	BY
JOB NO:	2222191
DATE:	09-25-23
SCALE:	AS NOTED
DESIGN BY:	DM
CHECKED BY:	CP
SHEET NO:	







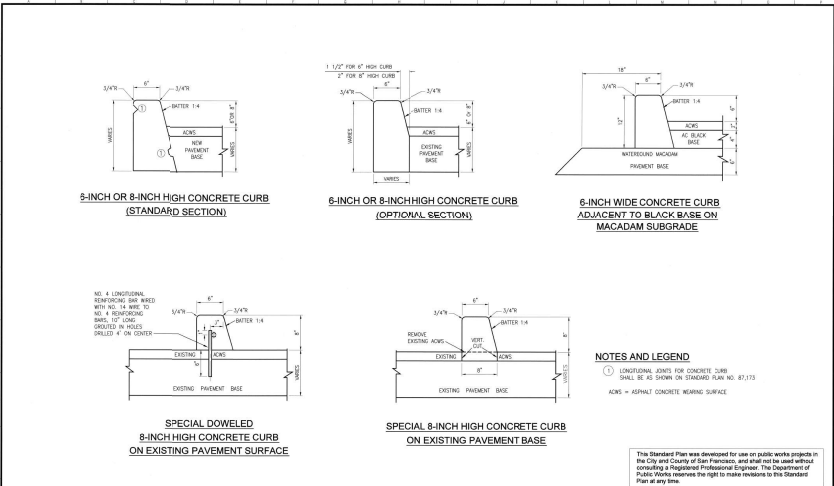
**LEGEND**

- NEW ASPHALT CONCRETE WEARING SURFACE (SLOPE)
- NEW TO BE COLD PLANED AND RESURFACED
- EXISTING PAVEMENT OR FACILITY

This Standard Plan was developed for use on public works projects in the City and County of San Francisco, and shall not be used without consulting a Registered Professional Engineer. The Department of Public Works reserves the right to make revisions to this Standard Plan at any time.

DATE: 04/18/2007	BY: [Signature]	APPROVED: [Signature]	STANDARD PLAN	NO. 11-1	40.000
DATE: 04/18/2007	BY: [Signature]	APPROVED: [Signature]	STANDARD COLD PLANING AND RESURFACING CROSS SECTIONS	NO. 11-1	83.168

**1 STANDARD COLD PLANING AND RESURFACING CROSS SECTIONS**  
NTS



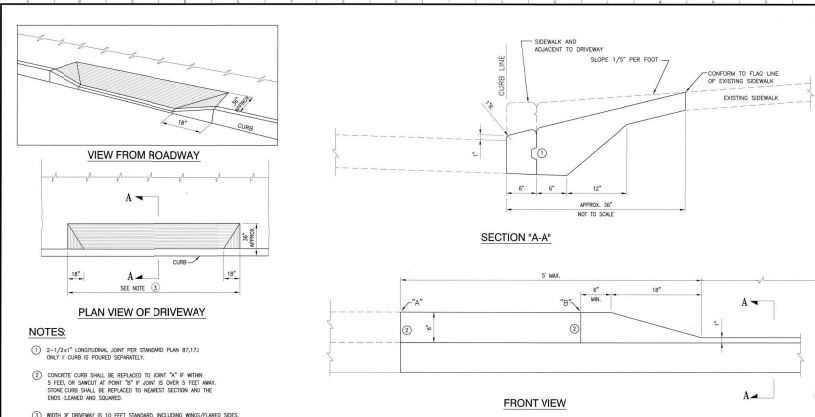
**NOTES AND LEGEND**

- OPTIONAL JOINTS FOR CONCRETE CURB SHALL BE AS SHOWN ON STANDARD PLAN NO. 87.113
- ACWS = ASPHALT CONCRETE WEARING SURFACE

This Standard Plan was developed for use on public works projects in the City and County of San Francisco, and shall not be used without consulting a Registered Professional Engineer. The Department of Public Works reserves the right to make revisions to this Standard Plan at any time.

DATE: 04/18/2007	BY: [Signature]	APPROVED: [Signature]	STANDARD PLAN	NO. 11-1	40.000
DATE: 04/18/2007	BY: [Signature]	APPROVED: [Signature]	STANDARD CURBS	NO. 11-1	87.169

**2 STANDARD CURBS**  
NTS



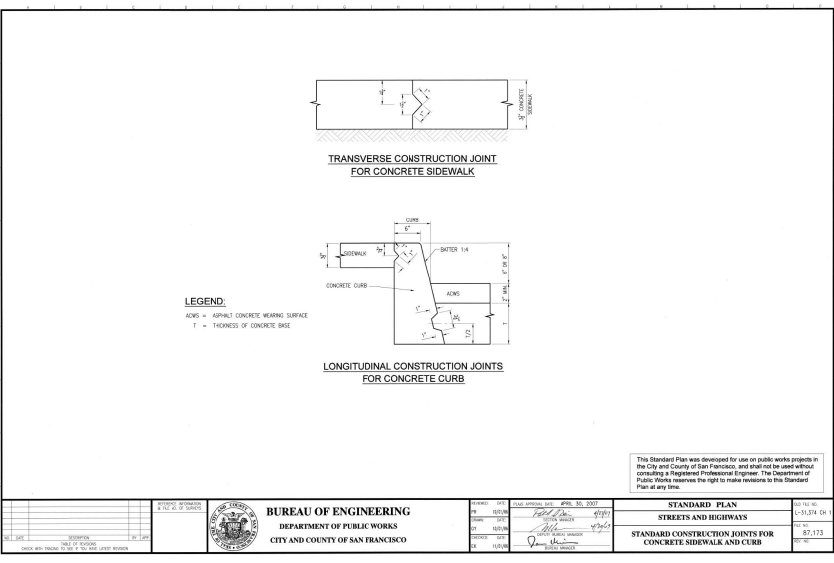
**NOTES**

- 2'-1/2" LONGITUDINAL JOINT PER STANDARD PLAN 87.173. ONLY IF CURB IS FORMED SEPARATELY.
- CONCRETE CURB SHALL BE REPLACED TO JOINT "A" IF WITHIN 5 FEET OR GABRIEL OF POINT "B" IF CURB IS OVER 5 FEET WIDE. STONE CURB SHALL BE REPLACED TO NEAREST SECTION AND THE ENDS LANDED AND SQUARED.
- NOTE IF DRAINAGE IS 10 FEET STANDARD INCLUDING MANHOLES/PLUMED TRENCHES.
- IF PROCEEDED WITH EXCESSIVE TO FEEL, BUT NO MORE THAN 30 FEET, THEN AND DRAINAGE MUST BE DEMONSTRATED ON BUILDING PLANS AND APPROVED BY THE PLANNING DEPARTMENT.
- A DRAWING EXCEEDING 30 FEET MAY BE REQUESTED BY APPLYING FOR AN INVOICE DRAWING PERMIT AT JMW.

This Standard Plan was developed for use on public works projects in the City and County of San Francisco, and shall not be used without consulting a Registered Professional Engineer. The Department of Public Works reserves the right to make revisions to this Standard Plan at any time.

DATE: 04/18/2007	BY: [Signature]	APPROVED: [Signature]	STANDARD PLAN	NO. 11-1	40.000
DATE: 04/18/2007	BY: [Signature]	APPROVED: [Signature]	STANDARD DRIVEWAY CONSTRUCTION	NO. 11-1	85.271

**3 STANDARD DRIVEWAY CONSTRUCTION**  
NTS



**LEGEND**

- ACWS = ASPHALT CONCRETE WEARING SURFACE
- T = THICKNESS OF CONCRETE BASE

This Standard Plan was developed for use on public works projects in the City and County of San Francisco, and shall not be used without consulting a Registered Professional Engineer. The Department of Public Works reserves the right to make revisions to this Standard Plan at any time.

DATE: 04/18/2007	BY: [Signature]	APPROVED: [Signature]	STANDARD PLAN	NO. 11-1	40.000
DATE: 04/18/2007	BY: [Signature]	APPROVED: [Signature]	STANDARD CONSTRUCTION JOINTS FOR CONCRETE SIDEWALK AND CURB	NO. 11-1	87.173

**4 STANDARD CONSTRUCTION JOINTS FOR CONCRETE SIDEWALK AND CURB**  
NTS



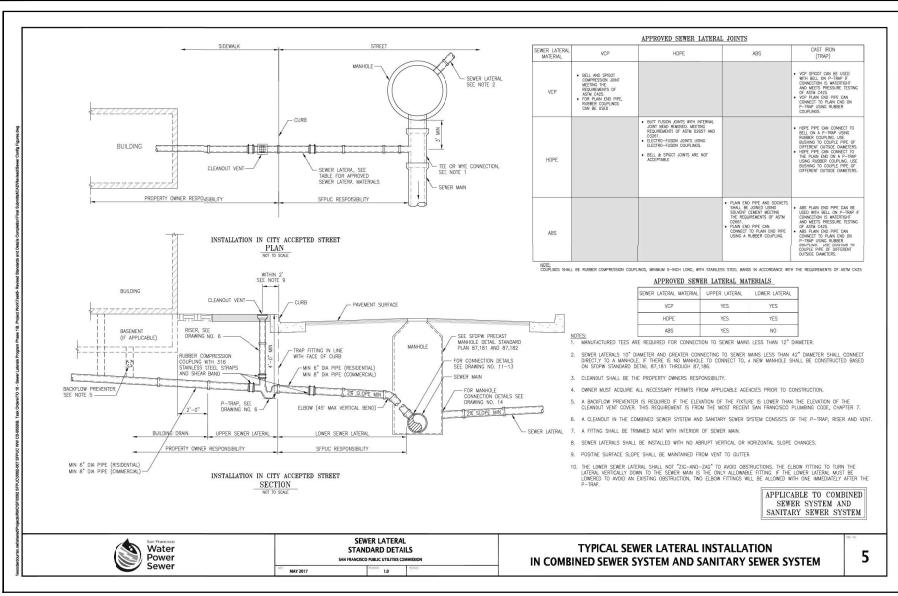
**LEA & BRAZE ENGINEERING, INC.**  
CIVIL ENGINEERS & LAND SURVEYORS  
RESIDENTIAL DESIGN  
DURHAM, NORTH CAROLINA  
HAYWARD, CALIFORNIA 94545  
(925) 887-6086  
WWW.LEABRAZE.COM

**LARCHIE & LAUDON**  
RESIDENCE  
551 ALVARADO STREET  
SAN FRANCISCO, CALIFORNIA  
APN: 3054-0339

**STREET IMPROVEMENTS  
DETAILS**

PLAN CHECK	DM
REVISIONS	BY
JOB NO:	2222191
DATE:	09-25-23
SCALE:	NTS
DESIGN BY:	DM
CHECKED BY:	CP
SHEET NO:	





1  
SI-4.1  
NTS  
**TYPICAL SEWER LATERAL INSTALLATION**



**LEA & BRAZE ENGINEERING, INC.**  
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**LARCHIE & LAUDON**  
 RESIDENCE  
 551 ALVARADO STREET  
 SAN FRANCISCO, CALIFORNIA  
 SAN FRANCISCO COUNTY  
 APN: 3674-0339

**STREET IMPROVEMENTS**  
 DETAILS

NO.	DATE	BY
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PLAN CHECK: DM  
 DATE: 03-27-24  
 REVISIONS: BY

JOB NO: 2222191  
 DATE: 09-25-23  
 SCALE: NTS  
 DESIGN BY: DM  
 CHECKED BY: CP  
 SHEET NO:



**CAUTION:**

- 1. CONTRACTOR SHALL CONTACT UNDERGROUND SERVICE ALERT FOR LOCATION OF UNDERGROUND UTILITIES AT LEAST 48 HOURS PRIOR TO COMMENCEMENT OF CONSTRUCTION...
2. THE LOCATION, SIZES AND/OR DEPTHS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS PLAN WERE OBTAINED FROM SOURCES OF VARYING RELIABILITY...
3. PRIOR TO COMMENCING WORKS, CONTRACTOR TO COORDINATE WITH SMTA'S TIM LIPP'S (415-819-1068) TO UNDERSTAND ALL RESTRICTIONS AND CLEARANCE REQUIREMENTS FOR WORKING IN THE VICINITY OF MUNI OVERHEAD CONTACT SYSTEM (OCS).

**GENERAL SITE NOTES:**

- 1. CONTRACTOR SHALL VISIT THE SITE PRIOR TO BIDDING ON THIS WORK AND CONSIDER THE EXISTING CONDITIONS AND SITE CONSTRAINTS IN THE BID...
2. ALL WORK IN THE PUBLIC RIGHT-OF-WAY SHALL CONFORM TO ALL APPLICABLE GOVERNING AGENCIES STANDARD DETAILS & SPECIFICATIONS.
3. PRIOR TO BEGINNING WORK, AND AFTER INITIAL HORIZONTAL CONTROL STAKING, CONTRACTOR SHALL FIELD CHECK ALL ELEVATIONS MARKED WITH (E) AND REPORT ANY DISCREPANCIES GREATER THAN 0.05' TO OWNER'S PROJECT MANAGER AND CIVIL ENGINEER.
4. ALL GENERAL NOTES, SHEET NOTES, AND LEGEND NOTES FOUND IN THESE DOCUMENTS SHALL APPLY TYPICALLY THROUGHOUT...
6. ALL WORK SHALL COMPLY WITH APPLICABLE CODES AND TRADE STANDARDS WHICH GOVERN EACH PHASE OF WORK INCLUDING, BUT NOT LIMITED TO, CALIFORNIA MECHANICAL CODE, CALIFORNIA PLUMBING CODE, CALIFORNIA ELECTRICAL CODE, CALIFORNIA FIRE CODE, CALTRANS STANDARDS AND SPECIFICATIONS, AND ALL APPLICABLE STATE AND/OR LOCAL CODES AND/OR LEGISLATION.
7. COMMENCEMENT OF WORK BY THE CONTRACTOR AND/OR ANY SUBCONTRACTOR SHALL INDICATE KNOWLEDGE AND ACCEPTANCE OF ALL CONDITIONS DESCRIBED IN THESE CONSTRUCTION DOCUMENTS, OR EXISTING ON SITE, WHICH COULD AFFECT THEIR WORK.
8. DAMAGE TO ANY EXISTING SITE IMPROVEMENTS, UTILITIES AND/OR SERVICES TO REMAIN SHALL BE RESPONSIBLE OF THE CONTRACTOR. CONTRACTOR SHALL REPAIR AND/OR REPLACE IN KIND.
9. CONTRACTOR SHALL REPLACE ALL STRUCTURES AND GRATE LIDS FOR VAULTS, CATCH BASINS, ETC., WITH VEHICULAR-RATED STRUCTURES IN ALL TRAFFIC ACCESSIBLE AREAS.
10. THE CONTRACTOR SHALL ADJUST TO FINAL GRADE ALL EXISTING AND/OR NEW MANHOLES, CURB INLETS, CATCH BASIN, VALVES, GROUND COVERS, AND OTHER CASTINGS WITHIN THE WORK AREA TO FINAL GRADE IN PAVEMENT AND LANDSCAPE AREAS UNLESS NOTED OTHERWISE.
11. CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT INCLUDING SAFETY OF ALL PERSONS AND PROPERTY THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS AND THAT THE CONTRACTOR SHALL DEFEND INDEMNIFY AND HOLD THE OWNER, THE CONSULTING ENGINEER AND THE CITY HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT...
12. EXISTING PEDESTRIAN WALKWAYS, BIKE PATHS AND ACCESSIBLE ACCESS PATHWAYS SHALL BE MAINTAINED, WHERE FEASIBLE, DURING CONSTRUCTION.
13. IF A CONFLICT ARISES BETWEEN THE SPECIFICATIONS AND THE PLANS NOTES, THE MORE STRINGENT REQUIREMENT SHALL GOVERN.
14. IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
15. THE DUTY OF THE ENGINEERS TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES, IN, ON OR NEAR THE CONSTRUCTION SITE.
16. NEITHER THE FINAL PAYMENT, NOR THE PROVISIONS IN THE CONTRACT, NOR PARTIAL NOR ENTIRE USE OR OCCUPANCY OF THE PREMISES BY THE OWNER SHALL CONSTITUTE AN ACCEPTANCE OF THE WORK NOT DONE IN ACCORDANCE WITH THE CONTRACT OR RELIEVES THE CONTRACTOR OF LIABILITY IN RESPECT TO ANY EXPRESS WARRANTIES OR RESPONSIBILITY FOR FAULTY MATERIAL OR WORKMANSHIP.
17. THE CONTRACTOR SHALL REMEDY ANY DEFECTS IN WORK AND PAY FOR ANY DAMAGE TO OTHER WORK RESULTING THERE FROM WHICH SHALL APPEAR WITHIN A PERIOD OF ONE (1) CALENDAR YEAR FROM THE DATE OF FINAL ACCEPTANCE OF THE WORK.

**EXISTING CONDITIONS:**

- 1. EXISTING TOPOGRAPHIC SURVEY FOR OFF-SITE PERFORMED BY LEA AND BRAZE ENGINEERING ON 07-27-18 (JOB #2180481.014 SU) GRADES ENCOUNTERED MAY VARY FROM THOSE SHOWN. CONTRACTOR SHALL REVIEW THE PLANS AND CONDUCT FIELD INVESTIGATIONS AS REQUIRED TO VERIFY EXISTING CONDITIONS AT THE PROJECT SITE.
2. FOR ALL TOPOGRAPHIC SURVEY BY OTHERS, CLIENT SHALL HOLD HARMLESS LEA & BRAZE ENGINEERING FROM ANY AND ALL OCCURRENCES RESULTING FROM THE ACCURACY/INACCURACY OF THE CLIENT SUPPLIED TOPOGRAPHIC AND BOUNDARY SURVEY (AS PREPARED BY OTHERS).

**SITE FENCING NOTES:**

- 1. CONTRACTOR SHALL PROVIDE A CONSTRUCTION FENCE AROUND THE ENTIRE AREA OF DEMOLITION AND CONSTRUCTION, INCLUDING ALL STAGING, STORAGE, CONSTRUCTION OFFICE AND LAYDOWN AREAS.
2. FENCE LOCATION MAY BE ADJUSTED FROM TIME TO TIME AS CONSTRUCTION PROCEEDS TO EXCLUDE SOME AREAS WHERE CONSTRUCTION WORK IS NOT BEING DONE AND THE AREA IS NOT OBJECTABLE IN VISUAL APPEARANCE, AT THE DISCRETION AND APPROVAL OF THE DISTRICT STAFF.
3. CONSTRUCTION FENCE SHALL BE A MINIMUM OF A 6' HIGH GALVANIZED CHAIN LINK FENCE WITH GREEN WINDSCREEN FABRIC ON THE OUTSIDE OF THE FENCE.
4. CONTRACTOR SHALL REPLACE THE GREEN FABRIC AT LEAST ONCE A YEAR OR AT SUCH A TIME AS IT BECOMES TATTERED AND UNSIGHTLY DUE TO WIND OR CONSTRUCTION ACTIVITIES.

**SURVEYOR'S NOTES:**

THE TYPES, LOCATIONS, SIZES AND/OR DEPTHS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS TOPOGRAPHIC SURVEY ARE APPROXIMATE AND WERE OBTAINED FROM SOURCES OF VARYING RELIABILITY. ONLY ACTUAL EXCAVATION WILL REVEAL THE TYPES, EXTENT, SIZES, LOCATIONS AND DEPTHS OF SUCH UNDERGROUND UTILITIES. A REASONABLE EFFORT HAS BEEN MADE TO LOCATE AND DELINEATE ALL KNOWN UNDERGROUND UTILITIES. HOWEVER, THE ENGINEER CAN ASSUME NO RESPONSIBILITY FOR THE COMPLETENESS OR ACCURACY OF ITS DELINEATE OF SUCH UNDERGROUND UTILITIES WHICH MAY BE ENCOUNTERED, BUT WHICH ARE NOT SHOWN ON THIS SURVEY. CONTRACTOR SHALL VERIFY ALL UTILITIES PRIOR TO CONSTRUCTION.

**HORIZONTAL CONTROL NOTES:**

- 1. CONTRACTOR SHALL LAYOUT THE CONTROL FOR THE SITE AS SPECIFIED ON HORIZONTAL CONTROL SHEET. CONTRACTOR SHALL CLEARLY SET AND MARK EACH OF THE CONTROL POINTS, PROTECTING THE POINTS THROUGHOUT CONSTRUCTION.
2. ALL DIMENSIONS ON THE PLANS ARE IN FEET OR DECIMALS THERE OF UNLESS SPECIFICALLY CALLED OUT AS FEET AND INCHES.
3. ALL BUILDINGS, DRIVEWAYS AND PARKING LOTS ARE TO BE PARALLEL AND RIGHT ANGLES TO THE BASSIS OF BEARINGS UNLESS IDENTIFIED OR CLEARLY SHOWN AS A DIFFERENT ANGLE.
4. ALL RETURN RADI AND CURB DATA ARE TO FACE OF CURB, UNLESS OTHERWISE SHOWN OR INDICATED.

**DEMOLITION NOTES:**

- 1. CONTRACTOR IS TO COMPLY WITH ALL GENERAL AND STATE REQUIREMENTS INVOLVING THE REMOVAL AND DISPOSAL OF HAZARDOUS MATERIAL(S).
2. THE CONTRACTOR SHALL LOCATE AND CLEARLY MARK (AND THEN PRESERVE THESE MARKERS) FOR THE DURATION OF CONSTRUCTION OF ALL TELEPHONE, DATA, STREET LIGHTING AND POWER FACILITIES THAT ARE IN OR NEAR THE AREA OF CONSTRUCTION.
3. CONTRACTOR'S BID IS TO INCLUDE ALL VISIBLE SURFACE AND ALL SUBSURFACE FEATURES IDENTIFIED TO BE REMOVED OR ABANDONED IN THESE DOCUMENTS.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR A SITE INSPECTION TO FULLY ACKNOWLEDGE THE EXTENT OF THE DEMOLITION WORK.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY AND ALL PERMITS NECESSARY FOR ENCROACHMENT, GRADING, DEMOLITION, AND STATE JURISDICTIONS.
6. THE CONTRACTOR SHALL PAY ALL FEES ASSOCIATED WITH DISPOSAL OF MATERIALS.
7. BACKFILL ALL DEPRESSIONS AND TRENCHES FROM DEMOLITION OF FOUNDATIONS & UTILITIES TO THE SATISFACTION OF THE GEOTECHNICAL ENGINEER.
8. WITHIN LIMITS OF WORK, REMOVE CURBS, GUTTERS, LANDSCAPING, SIGNAGE, TREES, SCRUBS, ASPHALT, UNDERGROUND PIPES, ETC. AS INDICATED ON THE PLANS AND SPECIFICATIONS.
9. REMOVAL OF LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIALS TO THE SATISFACTION OF THE GEOTECHNICAL ENGINEER.
10. PRIOR TO BEGINNING DEMOLITION WORK ACTIVITIES, CONTRACTOR SHALL INSTALL EROSION CONTROL MEASURES OUTLINED IN THE EROSION PLAN & DETAILS.
11. CONTRACTOR SHALL BE RESPONSIBLE FOR DISPOSING ALL DEMOLITION MATERIALS, OR STORING SELECTED ITEMS BY OWNER'S REPRESENTATIVE AT DESIGNATED LOCATIONS.
12. THE CONTRACTOR SHALL MAINTAIN ALL SAFETY DEVICES, AND SHALL BE RESPONSIBLE FOR CONFORMANCE TO ALL LOCAL, STATE AND FEDERAL SAFETY AND HEALTH STANDARDS LAWS AND REGULATIONS.
13. THE CONTRACTOR SHALL PROTECT FROM DAMAGE ALL EXISTING IMPROVEMENTS FACILITIES AND STRUCTURES WHICH ARE TO REMAIN, ANY ITEMS DAMAGED BY THE CONTRACTOR OR HIS AGENTS OF ANY ITEMS REMOVED FOR HIS USE SHALL BE REPLACED IN EQUAL OR BETTER CONDITION AS APPROVED BY THE ARCHITECT OR OWNER'S REPRESENTATIVE.
14. COORDINATE WITH ELECTRICAL, MECHANICAL, LANDSCAPING AND ARCHITECTURAL DRAWINGS FOR UTILITY SHUT-DOWN / DISCONNECT LOCATIONS. CONTRACTOR IS TO SHUT OFF ALL UTILITIES AS NECESSARY PRIOR TO DEMOLITION. CONTRACTOR IS TO COORDINATE SERVICE INTERRUPTIONS WITH THE DEVELOPER / OWNER. DO NOT INTERRUPT SERVICES ADJACENT OFF-SITE OWNERS. ALSO SEE ARCHITECTURAL PLANS FOR ADDITIONAL DEMOLITION SCOPE OF WORK.
15. DEMOLITION INCLUDES REMOVAL OF ALL ITEMS ASSOCIATED WITH THE UTILITY, RETAINING WALL, FENCE, TREE OR BUILDING, INCLUDING BUT NOT LIMITED TO FOOTINGS, VALVES, ROOTS, BACKFILL, ETC. AND SHALL INCLUDE PREPARING THE SITE FOR NEW UTILITIES, BUILDINGS, RETAINING WALLS, ETC.
16. ALL MATERIALS TO BE DEMOLISHED AND REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE LAWFULLY DISPOSED OF OFF-SITE.
17. THE PLAN IS NOT INTENDED TO BE A COMPLETE CATALOGUE OF ALL EXISTING STRUCTURES AND UTILITIES. THIS PLAN INTENDS TO DISCLOSE GENERAL INFORMATION KNOWN BY THE ENGINEER AND TO SHOW THE LIMITS OF THE AREA WHERE WORK WILL BE PERFORMED. THIS PLAN SHOWS THE EXISTING FEATURES TAKEN FROM A FIELD SURVEY, FIELD INVESTIGATION AND/OR DEMOLITION. THIS PLAN AND FIELD AREAS SHALL NOT ACCURATELY REFLECT THE TYPE OR EXTENT OF THE ITEMS TO BE ENCOUNTERED AS THEY ACTUALLY EXIST. WHERE EXISTING FEATURES ARE NOT SHOWN, IT IS IMPLIED THAT THEY ARE NOT TO BE DEMOLISHED OR REMOVED. THE CONTRACTOR SHALL PERFORM A THOROUGH FIELD INVESTIGATION AND REVIEW OF THE SITE WITHIN THE LIMIT OF WORK SHOWN IN THIS PLAN SET TO DETERMINE THE TYPE, QUANTITY AND EXTENT OF ANY AND ALL ITEMS. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR DETERMINING THE EXTENT OF EXISTING STRUCTURES AND UTILITIES AND QUANTITY OR WORK INVOLVED IN REMOVING THESE ITEMS FROM THE SITE.
18. ALL ABANDONED BUILDINGS AND FOUNDATIONS, TREE (EXCEPT THOSE SPECIFIED TO REMAIN FOR LANDSCAPING PURPOSES), VEGETATION AND ANY SURFACE DEBRIS SHALL BE REMOVED AND DISPOSAL OF OFF THE SITE BY THE CONTRACTOR.
19. ALL ABANDONED UTILITY LINES SHALL BE REMOVED OR DEMOLISHED. THE APPROPRIATE FINAL DISPOSITION OF SUCH LINES DEPEND UPON THEIR DEPTH AND LOCATION AND THE METHOD OF REMOVAL OR DEMOLITION SHALL BE DETERMINED BY THE SOILS ENGINEER. ONE OF THE FOLLOWING METHODS WILL BE USED:
A. EXCAVATE AND TOTALLY REMOVE THE UTILITY LINE FROM THE TRENCH.
B. EXCAVATE AND CRUSH THE UTILITY LINE IN THE TRENCH.
C. CAP THE ENDS OF THE UTILITY LINE WITH CONCRETE TO PREVENT THE ENTRANCE OF WATER. THE LOCATION AT WHICH THE UTILITY LINE WILL BE CAPPED WILL BE DETERMINED BY THE CITY ENGINEER. THE LENGTH OF THE CAP SHALL NOT BE LESS THAN FIVE FEET, AND THE CONCRETE MIX EMPLOYED SHALL HAVE MINIMUM SHRAGE.

**SANITARY SEWER NOTES:**

- 1. INSTALL DETECTABLE METALIZED WARNING TAPE APPROXIMATELY 6"-12" BELOW THE SERVICE IN NON-PAVED AREAS, AND AT THE BOTTOM OF BASEROCK FOR PAVED AREAS. TAPE SHALL BE IMPRINTED WITH "CAUTION-SANITARY SEWER LINE BELOW", CALCIPO TYPE 2 OR EQUAL.
2. ALL SEWER WORK SHALL BE IN CONFORMANCE WITH THE CITY AND COUNTY OF SAN FRANCISCO STANDARD SPECIFICATION SECTION 300.
3. PUBLIC AND PRIVATE SANITARY SEWER MAIN AND SERVICE LINE SHALL BE VITRIFIED CLAY PIPE (VCP).
4. WHERE CONNECTION IS TO BE MADE TO AN EXISTING SEWER OR STRUCTURE, SAID EXISTING SEWER OR STRUCTURE SHALL BE UNCOVERED AND CHECKED FOR LOCATION AND ELEVATION PRIOR TO STAKING NEW SEWER DEPTH AND LOCATION. ANY DISCREPANCY BETWEEN THE PLANS AND THE FIELD INFORMATION SHALL BE REPORTED IMMEDIATELY TO THE ENGINEER.
5. MINIMUM SLOPE FOR SITE SANITARY SEWER PIPES SHALL BE CURRENT UPC REQUIREMENTS:
4" 0.2%
6" 0.1%
8" & LARGER 0.05%
6. ALL LATERALS SHALL HAVE AN INVERT AT THE BACK OF CURB, AS SHOWN ON THE PLANS AND PER THE CITY AND COUNTY OF SAN FRANCISCO STANDARD PLANS.

**GRADING AND EARTHWORK NOTES:**

- 1. ALL PAVED AREAS ARE TO SLOPE AT A MINIMUM OF 1% ACCESSIBLE STALLS AND LOADING ZONES ARE TO SLOPE AT A MAXIMUM OF 2% IN ANY DIRECTION AND ACCESSIBLE DRIVEWAYS ARE TO SLOPE AT A MAXIMUM OF 5% WITH A MAXIMUM CROSS-SLOPE OF 2%. ANY AREAS ON THE SITE NOT CONFORMING TO THESE BASIC RULES DUE TO EXISTING CONDITIONS OR DISCREPANCIES IN THE DOCUMENTS ARE TO BE REPORTED TO OTHER ARCHITECT/ENGINEER PRIOR TO PROCEEDING WITH FORMWORK FOR CURBS AND/OR FLATWORK.
2. CONTRACTOR SHALL DETERMINE EARTHWORK QUANTITIES BASED ON THE TOPOGRAPHIC SURVEY, THE SOILS INVESTIGATION AND THE PROPOSED SURFACE GRADES AND BASE GRADES, ETC. ACCORDINGLY, ANY DIFFERENCES BETWEEN THE STATE OF THE PROJECT SITE DELIVERED TO THE CONTRACTOR AND THESE DOCUMENTS SHOULD BE NOTED TO THE CIVIL ENGINEER.
3. ALL FILL SHALL BE COMPACTED PER THE CONSTRUCTION SPECIFICATIONS AND THE CONTRACTOR SHALL COORDINATE AND COMPLY WITH THE OWNERS TESTING AGENCY TO TAKE THE APPROPRIATE TEST TO VERIFY COMPACTION VALUES.
4. IMPORT SOILS MUST MEET THE REQUIREMENTS OF THE SOILS REPORT AND SPECIFICATIONS.
5. COORDINATE THE PLACEMENT OF ALL SLEEVES FOR LANDSCAPE IRRIGATION (WATER AND CONTROL WIRING) AND STREET LIGHTING PRIOR TO THE PLACEMENT OF ANY ASPHALT, BASEROCK, OR CONCRETE SURFACING. SEE LANDSCAPE AND SITE ELECTRICAL DRAWINGS.
6. DO NOT ADJUST GRADES ON THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL OF ENGINEER.
7. SPOT ELEVATIONS ARE TO FINISHED SURFACE.
8. TOP OF CONCRETE CURBS ARE 0.60" ABOVE TOP OF PAVING ELEVATIONS, U.N.O.
9. ROUGH GRADING TO BE WITHIN 0.1' AND FINISH GRADES ARE TO BE WITH IN 0.05'.
10. AFTER STAKING FOR HORIZONTAL CONTROL CONTRACTOR SHALL FIELD CHECK ALL ELEVATIONS MARKED WITH (E) AND REPORT ANY DISCREPANCIES GREATER THEN 0.05' TO ARCH/ENGR.
11. ALL EXISTING UTILITY STRUCTURES WITHIN THE AREA OF WORK SHALL HAVE THE LIDS, GRATES, COVERS, ETC. ADJUSTED TO BE FLUSH WITH FINISHED GRADES. CONTRACTOR SHALL IDENTIFY ALL SUCH ITEMS BY USE OF THESE PLANS AND THOROUGH FIELD INVESTIGATION.
12. GEOTECHNICAL CONSULTANT TO BE NOTIFIED OF DELIVERY OF ALL IMPORTED SOILS TO SITE FOR HIS/HER INSPECTION AND APPROVAL PRIOR TO PLACING BY CONTRACTOR.
13. THESE SPECIFICATIONS AND APPLICABLE PLANS PERTAIN TO AND INCLUDE ALL SITE GRADING AND EARTHWORK ASSOCIATED WITH THE PROJECT INCLUDING, BUT NOT LIMITED TO THE FURNISHING OF ALL LABOR, TOOLS AND EQUIPMENT NECESSARY FOR SITE CLEARING AND GRUBBING, SITE PREPARATION, DISPOSAL OF EXCESS OR UNSUITABLE MATERIAL, STRIPPING, KEYING, EXCAVATION, OVER EXCAVATION, RECOMPACTION PREPARATION FOR SOIL RECEIVING FILL, PAVEMENT, FOUNDATION OF SLABS, EXCAVATION, IMPORTATION OF ANY REQUIRED FILL MATERIAL, PROCESSING, PLACEMENT AND COMPACTION OF FILL AND SUBSIDIARY WORK NECESSARY TO COMPLETE THE GRADING TO CONFORM TO THE LINES, GRADING AND SLOPE SHOWN ON THE PROJECT GRADING PLANS.
14. ALL FILL MATERIALS SHALL BE DENSIFIED SO AS TO PRODUCE A DENSITY NOT LESS THAN 90% RELATIVE COMPACTION BASED UPON ASTM TEST DESIGNATION D1557. FIELD DENSITY TEST WILL BE PERFORMED IN ACCORDANCE WITH ASTM TEST DESIGNATION 2922 AND 2017. THE LOCATION AND FREQUENCY OF THE FIELD DENSITY TEST WILL BE AS DETERMINED BY THE SOIL ENGINEER. THE RESULTS OF THE TESTS AND COMPLIANCE WITH THE SPECIFICATIONS WILL BE BASED UPON WHICH SATISFACTORY COMPLETION OF THE WORK WILL BE JUDGED BY THE SOIL ENGINEER. ALL CUT AND FILL SLOPES SHALL BE CONSTRUCTED AS SHOWN ON PLANS, BUT NO STEEPER THAN TWO (2) HORIZONTAL TO ONE (1) VERTICAL.
15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SATISFACTORY COMPLETION OF ALL THE EARTHWORK IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS. NO DEVIATION FROM THESE SPECIFICATIONS SHALL BE MADE EXCEPT UPON WRITTEN APPROVAL BY THE SOILS ENGINEER. BOTH CUT AND FILL AREAS SHALL BE SURFACE COMPLETED TO THE SATISFACTION OF THE SOILS ENGINEER AT THE CONCLUSION OF ALL OPERATIONS AND PRIOR TO FINAL ACCEPTANCE. THE CONTRACTOR SHALL NOTIFY THE SOILS ENGINEER AT LEAST TWO (2) WORKING DAYS PRIOR TO DOING ANY SITE GRADING AND EARTHWORK INCLUDING CLEARING.

**SIGNING & STRIPING NOTES:**

- 1. CONTRACTOR TO DOCUMENT ALL EXISTING STRIPINGS AND MARKINGS AFFECTED BY CONSTRUCTION ACTIVITIES, WITH PHOTOS. REPLACE IN KIND AFTER INSTALLATIONS OF ALL UTILITIES, CURBS, SIDEWALKS AND PAVING.
2. TRAFFIC SHALL BE MAINTAINED AT ALL TIMES AND SHALL BE PROTECTED WITH ADEQUATE BARRICADES, LIGHTS, SIGNS AND WARNING DEVICES AS PER THE CURRENT STATE OF CALIFORNIA, DEPARTMENT OF TRANSPORTATION, MANUAL OF TRAFFIC CONTROLS AND TO THE DIRECTION OF THE CITY'S INSPECTOR.

**SITE MAINTENANCE:**

- 1. REMOVE ALL DIRT, GRAVEL, RUBBISH, REFUSE, AND GREEN WASTE FROM STREET PAVEMENT AND STORM DRAINS ADJOINING THE SITE. LIMIT CONSTRUCTION ACCESS ROUTES ONTO THE SITE AND PLACE GRADE PADS AT THESE LOCATIONS. DO NOT DRIVE VEHICLES AND EQUIPMENT OFF THE PAVED OR GRAVELED AREAS DURING WET WEATHER.
2. SWEEP OR VACUUM THE STREET PAVEMENT AND SIDEWALKS ADJOINING THE PROJECT SITE AND THE ON-SITE PAVED AREAS ON A DAILY BASIS. SCRAPER CAKED-ON MUD AND DIRT FROM THESE AREAS BEFORE SWEEPING. CORNERS AND HARD TO REACH AREAS SHALL BE SWEEP MANUALLY.
3. CONTRACTOR SHALL GATHER ALL CONSTRUCTION DEBRIS ON A REGULAR BASIS AND PLACE IT IN A DUMPSTER OR OTHER CONTAINER WHICH IS EMPTIED OR REMOVED ON A REGULAR BASIS. PROVIDE APPROPRIATELY USE TAPPS ON THE GROUND TO COLLECT FALLEN DEBRIS OR SPLATTERS THAT COULD CONTRIBUTE TO STORM WATER RUNOFF POLLUTION.
4. IF THE STREET, SIDEWALKS AND/OR PARKING LOT ARE PRESSURE WASHED, DEBRIS MUST BE TRAPPED AND COLLECTED TO PREVENT ENTRY INTO THE STORM DRAIN SYSTEM. NO CLEANING AGENT MAY BE DISCHARGED INTO THE STORM DRAIN. IF ANY CLEANING AGENT OR DEGREASER IS USED, WASHED WATER MUST BE COLLECTED AND DISCHARGED TO THE SANITARY SEWER, SUBJECT TO THE APPROVAL OF THE OWNER'S PROJECT MANAGER, OR OTHERWISE DISPOSED THROUGH APPROVED DISPOSAL METHODS.
5. CREATE A CONTAINED AND COVERED AREA ON THE SITE FOR THE STORAGE OF BAGS, GEMENT, PAINTS, OILS, FERTILIZERS, PESTICIDES, OR OTHER MATERIAL USED ON THE SITE THAT HAVE THE POTENTIAL OF BEING WIND-BLOWN OR IN THE EVENT OF A MATERIAL SPILL.
6. NEVER CLEAN MACHINERY, EQUIPMENT OR TOOLS INTO A STREET, GUTTER OR STORM DRAIN.
7. ENSURE THAT CEMENT TRUCKS, PAINTERS, OR STUCCO/PLASTER FINISHING CONTRACTORS DO NOT DISCHARGE WASH WATER FROM EQUIPMENT, TOOLS OR RINSE CONTAINERS INTO GUTTERS OR DRAINS.
8. THE ON-SITE STORM DRAIN FACILITIES SHALL BE CLEANED A MINIMUM OF TWICE A YEAR AS FOLLOWS: IMMEDIATELY PRIOR TO OCTOBER 15TH AND ONCE IN JANUARY. ADDITIONAL CLEANING MAY BE REQUIRED IF FOUND NECESSARY BY THE CITY ENGINEER/INSPECTOR. CONTRACTOR SHALL BE RESPONSIBLE FOR COST ASSOCIATED WITH CLEANING.
9. PREVENT DUST FROM LEAVING THE SITE AND ACCUMULATING ON ADJACENT AREAS AS REQUIRED IN THE DUST CONTROL NOTES ON THIS SHEET.
10. PREVENT SEDIMENT LADEN STORM RUN-OFF FROM LEAVING THE SITE OR ENTERING STORM DRAIN OR SANITARY SEWER SYSTEMS AS REQUIRED IN THE EROSION AND SEDIMENTATION CONTROL NOTES ON THIS SHEET.
11. MAINTAIN EXISTING TREES AND PLANTS THAT ARE TO REMAIN AS REQUIRED BY THE TREE AND PLANT PROTECTION NOTES ON THE SHEET.

**PAVEMENT SECTION:**

- 1. SEE DETAIL SHEETS FOR ALL PAVEMENT SECTIONS
2. SEE STRUCTURAL DRAWINGS FOR BUILDING SLAB SECTIONS AND PAD PREPARATIONS.
3. SEE GRADING AND DETAIL SHEETS FOR FLATWORK SECTIONS AND BASE REQUIREMENTS.
4. EXISTING PAVEMENT SHALL BE TACK COATED PRIOR TO CONSTRUCTING NEW PAVEMENT.
5. THE FINAL OR SURFACE LAYER OF ASPHALT CONCRETE SHALL NOT BE PLACED UNTIL ALL ON-SITE IMPROVEMENTS HAVE BEEN COMPLETED, INCLUDING ALL GRADING AND ALL UNACCEPTABLE CONCRETE WORK HAS BEEN REMOVED AND REPLACED, UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER AND/OR DEVELOPER'S CITY ENGINEER.
6. ALL PAVING SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF CALTRANS STANDARD SPECIFICATIONS.

**EARTHWORK QUANTITY NOTES:**

- 1. THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE QUANTITIES OF ALL FORMS OF EARTHWORK ON THIS PROJECT AND BASING THE BID ON THOSE QUANTITIES WITH FULL KNOWLEDGE THAT ADDITIONAL PROCESSES - INCLUDING ENGINEERING - AND QUANTITIES ARE ALSO TO BE INCLUDED IN THE BID PER THE FOLLOWING NOTES.
2. THE CONTRACTOR SHALL MAKE AN INITIAL DETERMINATION OF THE QUANTITIES, BASED ON A DETAILED SITE VISIT, THE TOPOGRAPHIC SURVEY, THE GEOTECHNICAL REPORT, THE FINISH GRADES SHOWN ON THESE DRAWINGS, THE SIZE AND EXTENT OF FOOTINGS, THE PREPARATION AND MATERIALS USED FOR BUILDING SLABS, PAVEMENT SECTIONS, AND THE SIZE AND DEPTH OF UTILITY TRENCHES, INCLUDING THE UTILITIES CONTRACTOR ANTICIPATED RE-USE OF EXISTING MATERIAL FOR BACKFILL IF ANY.
3. THE CONTRACTOR SHALL MEET THE GRADES SHOWN ON THE DRAWINGS, ADJUSTING THE AMOUNT OF IMPORT OR EXPORT AS REQUIRED TO DO SO. NO ASSUMPTIONS SHOULD BE MADE ABOUT THE SITE BALANCING. NO ADJUSTMENTS TO THE GRADE SHALL BE PERMITTED UNLESS SPECIFICALLY APPROVED BY THE ARCH/ENGR IN WRITING AFTER THE IMPACT OF ANY GRADE CHANGES (IMPACT TO RAISED STAIRS, WORK BY OTHERS, ETC.) HAS BEEN THOROUGHLY REVIEWED BY THE ARCH/ENGR. WHEN PREPARING THE EARTHWORK BIDS, DO NOT ASSUME ANY CHANGES TO THE FINISHED GRADES SHOWN ON THESE DRAWINGS WILL BE PERMITTED.

**RECORD DRAWINGS:**

- 1. THE CONTRACTOR SHALL KEEP UP-TO-DATE AND ACCURATE A COMPLETE RECORD SET OF PRINTS OF THE CONTRACT DRAWINGS SHOWING EVERY CHANGE FROM THE ORIGINAL DRAWINGS MADE DURING THE COURSE OF CONSTRUCTION INCLUDING EXACT FINAL LOCATION, ELEVATION, SIZES, MATERIALS AND DESCRIPTION OF ALL WORK. RECORDS SHALL BE "REDLINED" ON A SET OF CONSTRUCTION PLAN DRAWINGS. PRINTS SHALL BE SUBMITTED TO THE CITY ENGINEER AND DEVELOPER'S CIVIL ENGINEER PRIOR TO FINAL ACCEPTANCE FOR REVIEW AND APPROVAL BY THE CITY/TOWN ENGINEER AND DEVELOPER'S CIVIL ENGINEER.



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STREET IMPROVEMENTS
STREET GRADING
SPECIFICATIONS

Table with 4 columns: PLAN CHECK, REVISIONS, DATE, SCALE, DESIGNED BY, CHECKED BY, SHEET NO. Includes a grid for revisions and project details.

**PURPOSE:**

THE PURPOSE OF THIS PLAN IS TO STABILIZE THE SITE TO PREVENT EROSION OF GRADED AREAS AND TO PREVENT SEDIMENTATION FROM LEAVING THE CONSTRUCTION AREA AND AFFECTING NEIGHBORING SITES, NATURAL AREAS, PUBLIC FACILITIES OR ANY OTHER AREA THAT MIGHT BE AFFECTED BY SEDIMENTATION. ALL MEASURES SHOWN ON THIS PLAN SHOULD BE CONSIDERED THE MINIMUM REQUIREMENTS NECESSARY TO MAINTAIN FIELD CONDITIONS DURING ADDITIONAL MEASURES, SUCH MEASURES SHALL BE PER CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENTATION CONTROL, AND THE CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION. LEA & BRAZE ENGINEERING SHOULD BE NOTIFIED IMMEDIATELY SHOULD CONDITIONS CHANGE.

**EROSION CONTROL NOTES:**

- IT SHALL BE THE OWNER'S/CONTRACTOR'S RESPONSIBILITY TO MAINTAIN CONTROL OF THE ENTIRE CONSTRUCTION OPERATION AND TO KEEP THE ENTIRE SITE IN COMPLIANCE WITH THIS EROSION CONTROL PLAN.
- THE INTENTION OF THIS PLAN IS FOR INTERIM EROSION AND SEDIMENT CONTROL ONLY. ALL EROSION CONTROL MEASURES SHALL CONFORM TO CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENTATION CONTROL, THE CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION, AND THE LOCAL GOVERNING AGENCY FOR THIS PROJECT.
- OWNER/CONTRACTOR SHALL BE RESPONSIBLE FOR MONITORING EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO, DURING, AND AFTER STORM EVENTS. PERSON IN CHARGE OF MAINTAINING EROSION CONTROL MEASURES SHOULD WATCH LOCAL WEATHER REPORTS AND ACT APPROPRIATELY TO MAKE SURE ALL NECESSARY MEASURES ARE IN PLACE.
- SANITARY FACILITIES SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
- DURING THE RAINY SEASON, ALL PAVED AREAS SHALL BE KEPT CLEAR OF EARTH MATERIAL AND DEBRIS. THE SITE SHALL BE MAINTAINED SO AS TO MINIMIZE SEDIMENT-LADEN RUNOFF TO ADJACENT STREETS, INCLUDING EXISTING DRAINAGE SWALES AND WATERCOURSES.
- CONSTRUCTION OPERATIONS SHALL BE CARRIED OUT IN SUCH A MANNER THAT EROSION AND WATER POLLUTION WILL BE MINIMIZED. COMPLIANCE WITH FEDERAL, STATE AND LOCAL LAWS CONCERNING POLLUTION SHALL BE MAINTAINED AT ALL TIMES.
- CONTRACTOR SHALL PROVIDE DUST CONTROL AS REQUIRED BY THE APPROPRIATE FEDERAL, STATE AND LOCAL AGENCY REQUIREMENTS.
- ALL MATERIALS NECESSARY FOR THE APPROVED EROSION CONTROL MEASURES SHALL BE IN PLACE BY OCTOBER 15TH.
- EROSION CONTROL SYSTEMS SHALL BE INSTALLED AND MAINTAINED THROUGHOUT THE RAINY SEASON OR FROM OCTOBER 15TH THROUGH APRIL 15TH, WHICHEVER IS LONGER.
- IN THE EVENT OF RAIN, ALL GRADING WORK IS TO CEASE IMMEDIATELY AND THE SITE IS TO BE SEALED IN ACCORDANCE WITH THE APPROVAL EROSION CONTROL MEASURES AND APPROVED EROSION CONTROL PLAN.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CHECKING AND REPAIRING EROSION CONTROL SYSTEMS AFTER EACH STORM.
- ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED BY LOCAL JURISDICTION'S ENGINEERING DEPARTMENT OR BUILDING OFFICIALS.
- MEASURES SHALL BE TAKEN TO COLLECT OR CLEAN ANY ACCUMULATION OR DEPOSIT OF DIRT, MUD, SAND, ROCKS, GRAVEL OR DEBRIS ON THE SURFACE OF ANY STREET, ALLEY OR PUBLIC PLACE OR IN ANY PUBLIC STORM DRAIN SYSTEM. REMOVAL OF ACCUMULATION SHALL BE DONE BY STREET SWEEPING OR HAND SWEEPING. WATER SHALL NOT BE USED TO WASH SEDIMENTS INTO PUBLIC OR PRIVATE DRAINAGE FACILITIES.
- EROSION CONTROL MEASURES SHALL BE ON-SITE FROM SEPTEMBER 15TH THRU APRIL 15TH.
- ALL EROSION CONTROL MEASURES SHALL BE INSTALLED AND MAINTAINED THROUGHOUT THE RAINY SEASON OR FROM OCTOBER 1 THROUGH APRIL 1, WHICHEVER IS GREATER.
- PLANS SHALL BE DESIGNED TO MEET C3 REQUIREMENTS OF THE MUNICIPAL STORMWATER REGIONAL PERMIT ("MRP") NPDES PERMIT CAS 012008.
- THE CONTRACTOR TO NPDES (NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM) BEST MANAGEMENT PRACTICES (BMP) FOR SEDIMENTATION PREVENTION AND EROSION CONTROL TO PREVENT DELETERIOUS MATERIALS OR POLLUTANTS FROM ENTERING THE TOWN OR COUNTY STORM DRAIN SYSTEMS.
- THE CONTRACTOR MUST INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO THE INCEPTION OF ANY WORK ON-SITE AND MAINTAIN THE MEASURES UNTIL THE COMPLETION OF ALL LANDSCAPING.
- THE CONTRACTOR SHALL MAINTAIN ADJACENT STREETS IN A NEAT, CLEAN DUST FREE AND SANITARY CONDITION AT ALL TIMES AND TO THE SATISFACTION OF THE TOWN INSPECTOR. THE ADJACENT STREET SHALL AT ALL TIMES BE KEPT CLEAR OF DEBRIS, WITH DUST AND OTHER NUISANCE BEING CONTROLLED AT ALL TIMES. THE CONTRACTOR BE RESPONSIBLE FOR ANY CLEAN UP ON ADJACENT STREETS AFFECTED BY THE BY THEIR CONSTRUCTION. METHOD OF STREET CLEANING SHALL BE BY DRY SWEEPING OF ALL PAVED AREAS. NO STOCKPILING OF BUILDING MATERIALS WITHIN THE TOWN RIGHT-OF-WAY.
- SEDIMENTS AND OTHER MATERIALS SHALL NOT BE TRACKED FROM THE SITE BY VEHICLE TRAFFIC. THE CONTRACTOR SHALL INSTALL A STABILIZED CONSTRUCTION ENTRANCE PRIOR TO THE INCEPTION OF ANY WORK ON-SITE AND MAINTAIN IT FOR THE DURATION OF THE CONSTRUCTION PROCESS SO AS TO NOT INHIBIT SEDIMENTS FROM BEING DEPOSITED INTO THE PUBLIC RIGHT-OF-WAY UNTIL THE COMPLETION OF ALL LANDSCAPING.
- THE CONTRACTOR SHALL PROTECT DOWN SLOPE DRAINAGE COURSES, STREAMS AND STORM DRAINS WITH ROCK FILLED SAND BAGS, TEMPORARY SWALES, SILT FENCES, AND EARTH PERMS IN CONJUNCTION OF ALL LANDSCAPING.
- STOCKPILED MATERIALS SHALL BE COVERED WITH VISQUEEN OR A TARPALIN UNTIL THE MATERIAL IS REMOVED FROM THE SITE. ANY REMAINING BARE SOIL THAT EXISTS AFTER THE STOCKPILE HAS BEEN REMOVED SHALL BE COVERED UNTIL A NATURAL GROUND COVER IS ESTABLISHED OR IT IS SEEDED OR PLANTED TO PROVIDE GROUND COVER PRIOR TO THE FALL RAINY SEASON.
- EXCESS OR WASTE CONCRETE MUST NOT BE WASHED INTO THE PUBLIC RIGHT-OF-WAY OR ANY OTHER DRAINAGE SYSTEM. PROVISIONS SHALL BE MADE TO RETAIN CONCRETE WASTES ON SITE UNTIL THEY CAN BE DISPOSED OF AS SOLID WASTE.
- TRASH AND CONSTRUCTION RELATED SOLID WASTES MUST BE DEPOSITED INTO A COVERED RECEPTACLE TO PREVENT CONTAMINATION AND DISPERSAL BY WIND

**EROSION CONTROL NOTES CONTINUED:**

- FUELS, OILS, SOLVENTS AND OTHER TOXIC MATERIALS MUST BE STORED IN ACCORDANCE WITH THEIR LISTING AND ARE NOT TO CONTAMINATE THE SOIL AND SURFACE WATERS. ALL APPROVED STORAGE CONTAINERS ARE TO BE PROTECTED FROM THE WEATHER. SPILLS MUST BE CLEANED UP IMMEDIATELY AND DISPOSED OF IN A PROPER MANNER. SPILLS MUST NOT BE WASHED INTO THE DRAINAGE SYSTEM.
- DUST CONTROL SHALL BE DONE BY WATERING AND AS OFTEN AS REQUIRED BY THE TOWN INSPECTOR.
- SILT FENCE(S) AND/OR FIBER ROLL(S) SHALL BE INSTALLED PRIOR TO SEPTEMBER 15TH AND SHALL REMAIN IN PLACE UNTIL THE LANDSCAPING GROUND COVER IS INSTALLED. CONTRACTOR SHALL MONITOR THESE MEASURES, FOLLOWING AND DURING ALL RAIN EVENTS TO PUBLIC OWNED FACILITIES.

**EROSION CONTROL MEASURES:**

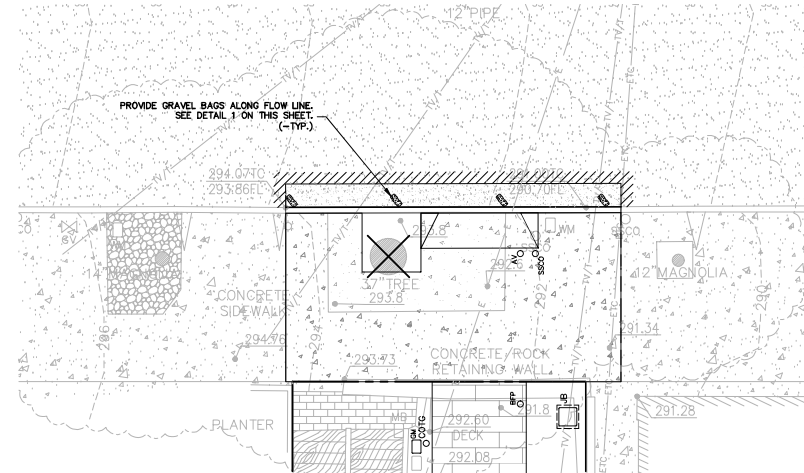
- THE FACILITIES SHOWN ON THIS PLAN ARE DESIGNED TO CONTROL EROSION AND SEDIMENT DURING THE RAINY SEASON, OCTOBER 15TH TO APRIL 15TH. EROSION CONTROL FACILITIES SHALL BE IN PLACE PRIOR TO OCTOBER 15TH OF ANY YEAR. GRADING OPERATIONS DURING THE RAINY SEASON WHICH LEAVE DENuded SLOPES SHALL BE PROTECTED WITH EROSION CONTROL MEASURES IMMEDIATELY FOLLOWING GRADING ON THE SLOPES.
- SITE CONDITIONS AT TIME OF PLACEMENT OF EROSION CONTROL MEASURES WILL VARY. APPROPRIATE ACTION INCLUDING TEMPORARY SWALES, INLETS, HYDROSEEDING, STRAW BALES, ROCK SACKS, ETC. SHALL BE TAKEN TO PREVENT EROSION AND SEDIMENTATION FROM LEAVING SITE. EROSION CONTROL MEASURES SHALL BE ADJUSTED AS THE CONDITIONS CHANGE AND THE NEED OF CONSTRUCTION SHIFT.
- CONSTRUCTION ENTRANCES SHALL BE INSTALLED PRIOR TO COMMENCEMENT OF GRADING. ALL CONSTRUCTION TRAFFIC ENTERING ONTO THE PAVED ROADS MUST CROSS THE STABILIZED CONSTRUCTION ENTRANCE. CONTRACTOR SHALL MAINTAIN STABILIZED ENTRANCE AT EACH VEHICLE ACCESS POINT TO EXISTING PAVED STREETS. ANY MUD OR DEBRIS TRACKED ONTO PUBLIC STREETS SHALL BE REMOVED DAILY AND AS REQUIRED BY THE GOVERNING AGENCY.
- ALL EXPOSED SLOPES THAT ARE NOT VEGETATED SHALL BE HYDROSEEDED. IF HYDROSEEDING IS NOT USED OR IS NOT EFFECTIVE BY OCTOBER 15, THEN OTHER IMMEDIATE METHODS MUST BE IMPLEMENTED, SUCH AS EROSION CONTROL BLANKETS, OR A THREE-STEP APPLICATION OF 1) SEED, MULCH, FERTILIZER 2) BLOWN STRAW 3) TACKIFIER AND MULCH. HYDROSEEDING SHALL BE IN ACCORDANCE WITH THE PROVISIONS OF SECTION 20" EROSION CONTROL AND HIGHWAY PLANTING" OF THE STANDARD SPECIFICATION OF THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION, AS LAST REVISED. REFER TO THE EROSION CONTROL SECTION OF THE GRADING SPECIFICATIONS THAT ARE A PART OF THIS PLAN SET FOR FURTHER INFORMATION.
- INLET PROTECTION SHALL BE INSTALLED AT OPEN INLETS TO PREVENT SEDIMENT FROM ENTERING THE STORM DRAIN SYSTEM. INLETS NOT USED IN CONJUNCTION WITH EROSION CONTROL ARE TO BE BLOCKED TO PREVENT ENTRY OF SEDIMENT. MINIMUM INLET PROTECTION SHALL CONSIST OF A ROCK SACKS OR AS SHOWN ON THIS PLAN.
- THIS EROSION AND SEDIMENT CONTROL PLAN MAY NOT COVER ALL THE SITUATIONS THAT MAY ARISE DURING CONSTRUCTION DUE TO UNANTICIPATED FIELD CONDITIONS. VARIATIONS AND ADDITIONS MAY BE MADE TO THIS PLAN IN THE FIELD BY A REPRESENTATIVE OF LEA & BRAZE ENGINEERING SHALL PERFORM A FIELD REVIEW AND MAKE RECOMMENDATIONS AS NEEDED. CONTRACTOR IS RESPONSIBLE TO NOTIFY LEA & BRAZE ENGINEERING AND THE GOVERNING AGENCY OF ANY CHANGES.
- THE EROSION CONTROL MEASURES SHALL CONFORM TO THE LOCAL JURISDICTION'S STANDARDS AND THE APPROVAL OF THE LOCAL JURISDICTION'S ENGINEERING DEPARTMENT.
- STRAW ROLLS SHALL BE PLACED AT THE TOE OF SLOPES AND ALONG THE DOWN SLOPE PERIMETER OF THE PROJECT. SWALES SHALL BE PLACED AT 15 FOOT INTERVALS ON GRADED SLOPES. PLACEMENT SHALL RUN WITH THE CONTOURS AND ROLLS SHALL BE TIGHTLY END BUTTED. CONTRACTOR SHALL REFER TO MANUFACTURER'S SPECIFICATIONS FOR PLACEMENT AND INSTALLATION INSTRUCTIONS.

**REFERENCES:**

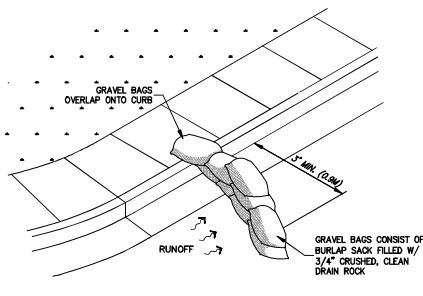
- CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD'S FIELD MANUAL FOR EROSION AND SEDIMENTATION CONTROL
- CALIFORNIA STORM WATER QUALITY ASSOCIATION BEST MANAGEMENT PRACTICES HANDBOOK FOR CONSTRUCTION

**PERIODIC MAINTENANCE:**

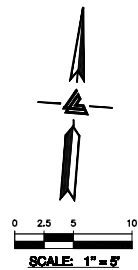
- MAINTENANCE IS TO BE PERFORMED AS FOLLOWS:
  - DAMAGES CAUSED BY SOIL EROSION OR CONSTRUCTION SHALL BE REPAIRED AT THE END OF EACH WORKING DAY.
  - SWALES SHALL BE INSPECTED PERIODICALLY AND MAINTAINED AS NEEDED.
  - SEDIMENT TRAPS, BERMS, AND SWALES ARE TO BE INSPECTED AFTER EACH STORM AND REPAIRS MADE AS NEEDED.
  - SEDIMENT SHALL BE REMOVED AND SEDIMENT TRAP RESTORED TO ITS ORIGINAL DIMENSIONS WHEN SEDIMENT HAS ACCUMULATED TO A DEPTH OF 1' FOOT.
  - SEDIMENT REMOVED FROM TRAP SHALL BE DEPOSITED IN A SUITABLE AREA AND IN SUCH A MANNER THAT IT WILL NOT ERODE.
  - RILLS AND GULLIES MUST BE REPAIRED.
- GRAVEL BAG INLET PROTECTION SHALL BE CLEANED OUT WHENEVER SEDIMENT DEPTH IS ONE HALF THE HEIGHT OF ONE GRAVEL BAG.
- STRAW ROLLS SHALL BE PERIODICALLY CHECKED TO ASSURE PROPER FUNCTION AND CLEANED OUT WHENEVER THE SEDIMENT DEPTH REACHED HALF THE HEIGHT OF THE ROLL.
- SILT FENCE SHALL BE PERIODICALLY CHECKED TO ASSURE PROPER FUNCTION AND CLEANED OUT WHENEVER THE SEDIMENT DEPTH REACHES ONE FOOT IN HEIGHT.
- CONSTRUCTION ENTRANCE SHALL BE REGRAVELED AS NECESSARY FOLLOWING SILT/SOIL BUILDUP.
- ANY OTHER EROSION CONTROL MEASURES SHOULD BE CHECKED AT REGULAR INTERVALS TO ASSURE PROPER FUNCTION



**EROSION CONTROL LEGEND**



**GRAVEL BAG AT STREET FLOW LINE**



**LEA & BRAZE ENGINEERING, INC.**  
 CIVIL ENGINEERS | LAND SURVEYORS  
 REGISTERED PROFESSIONAL ENGINEERS  
 1000 INDUSTRIAL PARK WEST  
 HAYWARD, CALIFORNIA 94545  
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**LARCHÉ & LAUDON**  
 RESIDENCE  
 551 ALVARADO STREET  
 SAN FRANCISCO, CALIFORNIA  
 APN: 3074-0339

**STREET IMPROVEMENTS  
 EROSION CONTROL  
 PLAN**

PLAN CHECK	DM
REVISIONS BY	
JOB NO:	2222191
DATE:	09-25-23
SCALE:	AS NOTED
DESIGN BY:	DM
CHECKED BY:	CP
SHEET NO:	



# PUBLIC COMMENT

**From:** [Donald Beilke](#)  
**To:** [BoardofAppeals \(PAB\)](#)  
**Cc:** [Rosenberg, Julie \(BOA\)](#)  
**Subject:** 551 Alvarado Street  
**Date:** Sunday, October 6, 2024 9:52:42 AM

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This message is from outside the City email system. Do not open links or attachments from untrusted sources.

Dear Members of the Board:

I am a San Francisco resident writing to ask you to please support the restoration and upgrading of the house on 551 Alvarado Street.

The upgrading and the addition of a one-car garage will not only enhance the property but the entire neighborhood as well.

Thank you for your support.

Sincerely,  
Donald Beilke

Sent from my iPhone

**From:** [Michael Hammond](#)  
**To:** [BoardofAppeals \(PAB\)](#)  
**Subject:** 551 Alvarado st.  
**Date:** Sunday, October 6, 2024 7:00:20 PM

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This message is from outside the City email system. Do not open links or attachments from untrusted sources.

Dear Board of Appeals

I'm writing in support of The restoration and improvements to 551 Alvarado st Home

I feel adding a garage to this house would

Make it a great home for families many years

And improve the neighborhood greatly.

Thank You for supporting this project

Michael Hammond

San Francisco

Sent from my iPhone

**From:** [Terry Micheau](#)  
**To:** [BoardofAppeals \(PAB\)](#)  
**Subject:** Appeal No. 24-050; 551 Alvarado Street - curb cut permit 23IE-00435  
**Date:** Monday, October 7, 2024 5:04:04 PM

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This message is from outside the City email system. Do not open links or attachments from untrusted sources.

Hello Board Members - Sorry not to be able to be present and testify on 10/9 when this item is heard but I wanted to weigh in as a neighbor to the south and also a 40 year resident of Noe Valley. Adding additional housing to this neighborhood is vital to its long term survival and growth so I urge the Board to deny the appeal of the curb cut that has apparently passed through all City agencies and is compliant with all relevant codes and rules.

Not all residents of Noe Valley drive cars and many who do utilize garages for parking their car. This home renovation provides at least one new off street garage spot and another at the curb under the approved design. The suggestion that a slanted driveway might allow for a second spot at the curb has been dismissed by an engineering study as not practical or drivable.

It is interesting to note that the appellant's driveway at 555 Alvarado is slanted in a similar fashion to the one studied at 551 Alvarado but the Google photo of 555 Alvarado that I found shows a car blocking the driveway, presumably that of the owner of 555 Alvarado. Maybe they are not able to negotiate the slanted drive and therefore park in their own driveway versus their garage? If so, this certainly does not help the extremely tight parking situation the appellant points to on her block. The proposed curb cut at 551 Alvarado might allow for better access for both 555 and 551 Alvarado. A win-win situation.

Please evaluate the above factors and I urge you to allow this worthy project to proceed by denying this late arriving appeal.

Thank you for your attention to this matter and please advise me of the hearing outcome.

Terry Micheau  
4048 23rd Street  
SF 94114